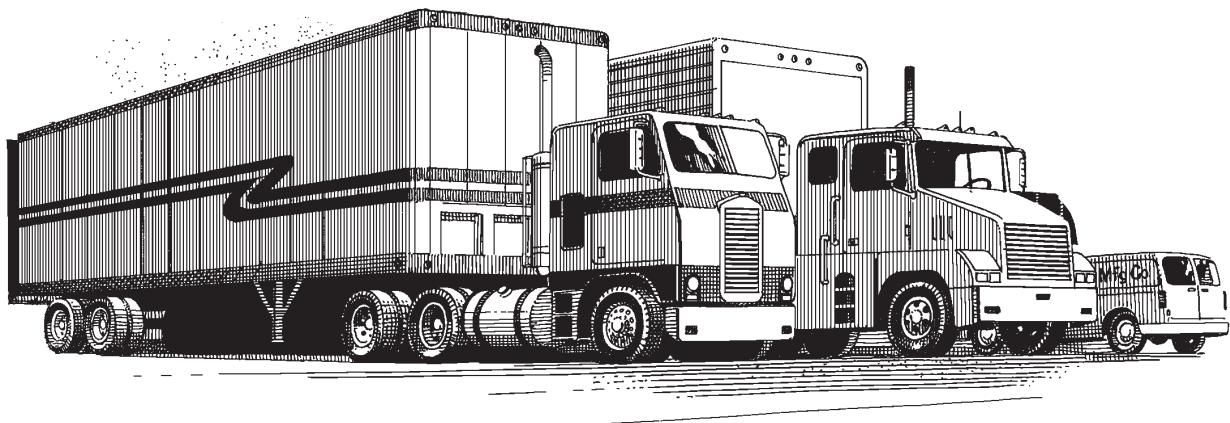


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Guide for Uniform Laws and Regulations Governing Truck Size and Weight Among the WASHTO States

Adopted by the WASHTO Policy Committee
March 2009



Western Association of State Highway and Transportation Officials

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The Western Association of Highway Transportation Officials (WASHTO) recognizes that the productivity of our transportation systems is the key to a viable national economy. They further recognize that the commercial users of our roadway system need assistance to navigate through the various administrative practices established by each jurisdiction to preserve the infrastructure and provide safety to the motoring public. In 1987, WASHTO Chief Administrative Officers adopted a resolution committing resources through the Subcommittee on Highway Transport (a full Committee since 1995) to work toward uniformity in truck size and weight issues among the WASHTO states.

The WASHTO resolution set in motion a unique and talent-laden approach to the problem of uniformity. Each WASHTO member jurisdiction, and each federal agency involved in vehicle size and weight, assigned experienced representatives to the Committee. In addition, the Committee invited representatives from industry to act as technical advisors. Industry representation has included the Western Trucking Association Executive Council, the Western Highway Institute and the Special Carriers and Riggers Association. Also included in Committee work has been the invaluable input from Canadian provincial representatives. The result has been an extremely dynamic and solution oriented group committed to making vehicle size and weight regulation as uniform as possible.

This “Guide” is one of the major products of the Committee. It is the result of continuous work performed by a variety of Committee focus groups, studying issues and providing recommendations for more unified approaches to common problems. The Guide provides recommended minimum standards for use by states in revising their regulatory process. By using the Guide, it is the hope of the Committee that industry will be able to move more easily between jurisdictions without eroding the necessary balance between freight, infrastructure and safety.

The Committee understands that each jurisdiction may require some exceptions based on road configurations and local issues. As a result, each jurisdiction maintains the right to develop special exceptions to these recommendations. The minimum standards, however, are expected to apply to the Interstate and primary routes, and any other road a state may determine as appropriate.

The Committee views the Guide as a “living document” and is committed to keeping it current, and to continue to report new recommendations deemed appropriate to enhance uniformity. This is the sixth edition of the Guide, first published in 1990, to be approved by the WASHTO Policy Committee.

The March 2009 update includes new personnel information, minor additions for clarity and consistency, cross-reference corrections, and grammatical corrections.

Mission Statement

The mission of the WASHTO Committee on Highway Transport in partnership with the motor carrier industry is to promote uniform laws, regulations, and practices among member jurisdictions and other jurisdictions for the efficient movement of goods and services while ensuring the safety of all highway users and preserving the highway infrastructure.

Goals

1. To promote uniformity among the WASHTO states and other jurisdictions regarding statutes, regulations, and ITS/CVO and their impacts on the transport industry and highway safety.
2. To serve as a forum to review AASHTO's proposed policies on highway transport issues from a WASHTO perspective.
3. To review and monitor research, proposals, and federal and state regulations related to highway transport as they impact WASHTO members and to promote appropriate truck size and weight research where needed to assist in the fulfillment of the mission of the committee.
4. To provide a forum to share industry best practices among the WASHTO states.
5. To maximize ITS/CVO applications for the safe and efficient movement of goods and services.
6. To provide communication with the motor carrier industry, chief DOT executive offices, other WASHTO committees, and other organizations regarding the work and the recommendations of this committee.

In this chapter, the following words and phrases have the meaning specified.

- 1.01 AASHTO Guide:** A publication entitled Guide for Maximum Dimensions and Weights of Motor Vehicles and for the Operation of Nondivisible Load Oversize and Overweight Vehicles, published by the American Association of State Highway and Transportation Officials.
- 1.02 Axle:** The common axis of rotation of one or more wheels whether power-driven or freely rotating, and whether in one or more segments, and regardless of the number of wheels carried thereon.
- 1.03 Bus:** A motor vehicle designated primarily for the transportation of persons rather than property and having a passenger-carrying capacity of more than 15 persons including the driver, other than a taxi cab constructed and designed for transporting persons for commercial purposes.
- 1.04 Combination Length:** The total length of a combination of vehicles, i.e., truck tractor-semitrailer-trailer combination, measured from front bumper of the motor vehicle to the rear extremity of the last trailer including the connecting tongue(s).
- 1.05 Combined Trailer Length:** The total length of a combination of trailers measured from the front of the first trailer to the rear extremity of the last trailer including the connecting tongue(s).
- 1.06 Gross Weight:** The weight of a vehicle and/or combination of vehicles plus the weight of any load thereon.
- 1.07 Height:** The total vertical dimension of a vehicle above the ground surface including any load.
- 1.08 Length:** The total longitudinal dimension of a single vehicle, a trailer, or a semitrailer. Length of a trailer or semitrailer is measured from the front of the cargo-carrying unit to its rear, exclusive of all overhang, safety or energy efficiency devices, including air conditioning units, air compressors, flexible fender extensions, splash and spray suppressant devices, bolsters, mechanical fastening devices, and hydraulic lift gates.
- 1.09 Longer Combination Vehicle:** A combination of truck tractor, semitrailer, and trailer(s) that exceeds legal length dimensions and operates on highways by permit for transporting reducible loads.

- 1.10 Manufactured Housing:** A structure constructed according to HUD/FHA mobile home construction and safety standards, transportable in one or more sections, which is built on a permanent chassis.
- 1.11 Modular Buildings:** A facility designed and constructed (of which does not include a permanent chassis) and is built to state building specifications.
- 1.12 Maximum Off Track:** The maximum difference in the path created by the center of the steering axle and the center of the rearmost axle of the vehicle or vehicle combination during the negotiation of a turn.
- 1.13 Motor Vehicle:** A vehicle which is self-propelled or propelled by electric power obtained from overhead trolley wires, but not operating upon rails.
- 1.14 National Network:** Those interstate and other federal-aid primary highways on which commercial vehicles of the dimensions authorized by the Surface Transportation Assistance Act of 1982 are allowed to operate.
- 1.15 New Generation Wide Base Single Tire:** A new generation wide base single tubeless tire shall have a nominal tire section width over 16 inches in width, and a ratio of tire sidewall height-to-width of less than or equal to 55 percent as marked on the second part of the number on the tire sidewall, e.g., 455/55R22.5, 445/50R22.5, 495/45R22.5.
- 1.16 Nondivisible Load:** A load which cannot be readily or reasonably dismantled and which is reduced to a minimum practical size and weight. Portions of a load can be detached and reloaded on the same hauling unit provided that the separate pieces are necessary to the operation of the machine or equipment that is being hauled, and if the arrangement does not exceed permitted weight limits.
- Federal definition of nondivisible load to be used for vehicles operating on the interstate is as follows: any load or vehicle exceeding applicable length or weight limits which, if separated into smaller loads or vehicles would: compromise the intended use of the vehicle, destroy the value of the load or vehicle, require more than eight work hours to dismantle using appropriate equipment.
- 1.17 Overhang:** Rear overhang, the distance from the center of the last axle to the end of the load, or portion of the vehicle whichever is longer.
- 1.18 Permit:** A written authorization to move or operate on a highway a vehicle or combination of vehicles with or without a load of size and/or weight exceeding the limits prescribed for vehicles in regular operation.

- 1.19 Pilot/Escort Vehicle:** A motor vehicle used for the express purpose as a warning vehicle for extra-legal vehicles.
- 1.20 Pounds Per Inch of Tire Width:** A measure of load restriction based on rated tire size. The pounds per inch of tire width are determined by dividing the weight carried by an axle group in pounds by the number of tires in the group and dividing that result by the manufacturers rated tire width as indicated on the sidewall of the tire.
- 1.21 Regional Permit:** Permits issued for movement of certain nonreducible overweight and/or oversize vehicles and/or loads on highways designated by the jurisdictions participating in the “Western Regional Agreement for the Issuance of Permits for Overweight and/or Oversize Vehicles and/or Loads Involved in Interstate Travel.”
- 1.22 Regular Operation:** The movement over highways of motor vehicles with dimensions and weights specified by state codes.
- 1.23 Retractable Axle:** An axle which can be separately raised and lowered by the driver of the vehicle but which may not have its weight bearing capacity regulated.
- 1.24 Rocky Mountain Double:** A combination of vehicles including a truck tractor pulling a long semitrailer and a shorter trailer.
- 1.25 Semitrailer:** Every single vehicle without motive power designed for carrying property and so designed in conjunction and used with a motor vehicle that some part of its own weight and that of its own load rests or is carried by another vehicle and having one or more load-carrying axles.
- 1.26 Single Axle:** An assembly of two or more wheels whose centers are in one transverse vertical plane or may be included between two parallel transverse planes 40 inches apart extending across the full width of the vehicle.
- 1.27 Single Unit:** A motor vehicle with no attached vehicles, i.e., truck, bus, truck tractor.
- 1.28 Steering Axle:** The axle or axles on the front of a motor vehicle that are activated by the operator to directly accomplish guidance or steering of the motor vehicle and/or combination of vehicles.
- 1.29 Super Single Wide Base Single Tire:** An original super single wide base tubeless tire shall have a nominal tire section width over 14 inches in width, and a ratio of tire sidewall height-to-width equal to 65 percent as marked in the second part of the number marked on the tire sidewall, e.g., 385/65R22.5,, 425/65R22.5, 445/65R22.5.

- 1.30 Superload:** A superload is any load that would require special approval by one or more state permit offices because of dimensions or weight. The guidelines contained in this manual reflect maximum size and weight limits for routine issuance of permits. Loads in excess of the stated guidelines would require additional coordination between industry representatives and state officials. Additionally, loads that do not exceed the superload guidelines may also require special approval by individual states. Refer to [Chapter 5.00](#) for suggested procedures, the superload map, and special conditions.
- 1.31 Tandem Axle:** Any two axles whose centers are more than 40 inches but not more than 96 inches apart and are individually attached to or articulated from (or both) a common attachment to the vehicle including a connecting mechanism designed to equalize the load between axles.
- 1.32 Trailer:** A single vehicle without motive power designed for carrying property wholly on its own structure, drawn by a motor vehicle, that carries no part of the weight and load of the trailer on its own wheels, and having two or more load carrying axles.
- 1.33 Tow Vehicle:** A motor vehicle used to transport manufactured housing.
- 1.34 Tridem Axle:** Any three consecutive axles whose extreme centers are not more than 144 inches apart and are individually attached to or articulated from (or both) a common attachment to the vehicle including a connecting mechanism designed to equalize the load between axles.
- 1.35 Triple Trailer Combination:** A combination of vehicles including a truck tractor pulling a semitrailer and two trailers.
- 1.36 Truck:** A single unit motor vehicle used primarily for the transportation of property.
- 1.37 Truck Tractor:** A motor vehicle used primarily for drawing other vehicles and not so constructed as to carry a load other than a part of the weight of the vehicle and load so drawn.
- 1.38 Trunnion Axle:** An axle configuration with two individual axles mounted in the same transverse plane, with four tires on each axle, connected at a pivot point which allows each individual axle to oscillate in a vertical plane to provide constant and equal weight distribution on each individual axle.
- 1.39 Trunnion Axle Group:** Two or more consecutive trunnion axles, which are individually attached to, and/or articulated from, the vehicle by a weight equalizing suspension system.

- 1.40 Turnpike Double:** A combination of vehicles including a truck tractor pulling a long semitrailer and an additional long trailer.
- 1.41 Variable Load Suspension (VLS) Axle:** An axle that can be separately raised and lowered by the driver of the vehicle. These axles are controlled by hydraulic or air suspension systems or by a combination of these methods.
- 1.42 Vehicle:** A device in, upon, or by which any person or property may be transported or drawn upon a highway, except devices moved by human power or used exclusively upon stationary rails or tracks.
- 1.43 Wide Base Tires:** Wide base single tires are tires whose nominal section width is over 14 inches.
- 1.44 Width:** The total outside transverse dimension of a vehicle including any load or load-holding devices thereon, but excluding approved safety devices and tire bulge due to load.

2.01 Scope: The provisions of this chapter governing width, height, length, and permissible loads should apply to vehicles serving in regular operation, without aid of special oversize/overweight permits. Vehicles operating under permit are covered in [Chapters 3.00, 4.00, 5.00, 6.00, 7.00, 8.00, and 9.00](#).

2.02 Width: Maximum vehicle width: 102 inches.

2.03 Height: Maximum vehicle height: 14 feet.

2.04 Length:

2.04.1 Single unit maximum length: 45 feet.

2.04.2 Truck-trailer combination maximum length: 75 feet.

2.04.3 Truck tractor-semitrailer combination maximum length: 65 feet when operated on highways not on the National Network.

2.04.4 Semitrailer maximum length: at least 53 feet on the National Network.

2.04.5 Combination length for a truck tractor-semitrailer-trailer combination: no maximum on National Network. Combined trailer length for a truck tractor-semitrailer-trailer combination: 61 feet. These provisions allow adequately for double 28-foot or double 28½-foot trailers.

2.05 Maximum Weights:

2.05.1 The maximum load per tire measured by pounds per inch of tire width shall be:

Steering Axle	600 lb/in
All other Axles	500 lb/in

Except for the steering axles or wide base single tires described below, all axles weighing more than 10,000 pounds shall have at least four tires per axle.

In lieu of four tires per axle, an axle may be equipped with wide base tires, limited to 500 pounds per inch of tire width.

Every state reserves the right to permit or otherwise establish limits in excess of those described above for use on vehicle configurations designed to address specific safety or economic concerns.

Each state shall determine effective dates based upon local economic, safety, and technological considerations. Amortization of investment and phasing out of single tires to minimize economic and operating disruption of individual companies and effected industries shall be considered factors.

2.05.2 Axle and Gross Weights: The maximum single axle weight shall be 20,000 pounds. The maximum tandem axle weight shall be 34,000 pounds. The maximum gross weight of a vehicle or combination of vehicles computed in accordance with 2.05.3, shall be 80,000 pounds.

2.05.3 Axle Group Weights: The total gross weight in pounds imposed on the highway by any group of two or more consecutive axles on a vehicle or combination of vehicles, shall not exceed the values computed by Federal Bridge Formula “B” as follows:

$$W = 500 ((LN/N-1)+12N+36)$$

Where: W = Maximum weight in pounds carried on any group of two or more axles computed to the nearest 500 pounds.

L = Distance in feet between the extremities of any group of two or more consecutive axles.

N = Number of axles in the group under consideration.

Exceptions to the axle group weights specified above which are federally mandated should be provided for by individual state code.

Bridge Formula Chart

Distance in Feet Between First and Last Axle of Any Group of Consecutive Axles

	2 Axles	3 Axles	4 Axles	5 Axles	6 Axles	7 Axles	8 Axles	9 Axles
4	34,000							
5	34,000							
6	34,000							
7	34,000							
8	34,000							
8+	38,000	42,000						
9	39,000	42,500						
10	40,000	43,500						
11		44,000						
12		45,000	50,000					
13		45,500	50,500					
14		46,500	51,500					
15		47,000	52,000					
16		48,000	52,500	58,000				
17		48,500	53,500	58,500				
18		49,500	54,000	59,000				
19		50,000	54,500	60,000				
20		51,000	55,500	60,500	66,000			
21		51,500	56,000	61,000	66,500			
22		52,500	56,500	61,500	67,000			
23		53,000	57,500	62,500	68,000			
24		54,000	58,000	63,000	68,500	74,000		
25		54,500	58,500	63,500	69,000	74,500		
26		55,500	59,500	64,000	69,500	75,000		
27		56,000	60,000	65,000	70,000	75,500		
28		57,000	60,500	65,500	71,000	76,500	82,000	
29		57,500	61,500	66,000	71,500	77,000	82,500	
30		58,500	62,000	66,500	72,000	77,500	83,000	
31		59,000	62,500	67,500	72,500	78,000	83,500	
32		60,000	63,500	68,000	73,000	78,500	84,500	90,000
33			64,000	68,500	74,000	79,000	85,500	90,500
34			64,500	69,000	74,500	80,000	85,500	91,000
35			65,500	70,000	75,000	80,500	86,000	91,500
36			68,000	70,500	75,500	81,000	86,500	92,000
37			68,000	71,000	76,000	81,500	87,000	93,000
38			68,000	71,500	77,000	82,000	87,500	93,500
39			68,000	72,500	77,500	82,500	88,500	94,000
40			68,500	73,000	78,000	83,500	89,000	94,500
41			69,500	73,500	78,500	84,000	89,500	95,000
42			70,000	74,000	79,000	84,500	90,000	95,500
43			70,500	75,000	80,000	85,000	90,500	96,000
44			71,500	75,500	80,500	85,500	91,000	96,500
45			72,000	76,000	81,000	86,000	91,500	97,500
46			72,500	76,500	81,500	87,000	92,500	98,000
47			73,500	77,500	82,000	87,500	93,000	98,500

	2 Axles	3 Axles	4 Axles	5 Axles	6 Axles	7 Axles	8 Axles	9 Axles
48			74,000	78,000	83,000	88,000	93,500	99,000
49			74,500	78,500	83,500	88,500	94,000	99,500
50			75,500	79,000	84,000	89,000	94,500	100,000
51			76,000	80,000	84,500	89,500	95,000	100,500
52			76,500	80,500	85,000	90,500	95,500	101,000
53			77,500	81,000	86,000	91,000	96,500	102,000
54			78,000	81,500	86,500	91,500	97,000	102,500
55			78,500	82,500	87,000	92,000	97,500	103,000
56			79,500	83,000	87,500	92,500	98,000	103,500
57			80,000	83,500	88,000	93,000	98,500	104,000
58				84,000	89,000	94,000	99,000	104,500
59				85,000	89,500	94,500	99,500	105,000
60				85,500	90,000	95,000	100,500	105,500
61				86,000	90,500	95,500	101,000	105,500
62				87,000	91,000	96,000	101,500	105,500
63				87,500	92,000	96,500	102,000	105,500
64				88,000	92,500	97,500	102,500	105,500
65				88,500	93,000	98,000	103,000	105,500
66				89,500	93,500	98,500	103,500	105,500
67				90,000	94,000	99,000	104,500	105,500
68				90,500	95,000	99,500	105,000	105,500
69				91,000	95,500	100,00	105,500	105,500
70				92,000	96,000	101,00	105,500	105,500
71				92,500	96,500	101,50	105,500	105,500
72				93,000	97,000	102,00	105,500	105,500
73				93,500	98,000	102,50	105,500	105,500
74				94,500	98,500	103,00	105,500	105,500
75				95,000	99,000	103,50	105,500	105,500
76				95,500	99,500	104,50	105,500	105,500
77				96,000	100,00	105,00	105,500	105,500
78				97,000	101,00	105,50	105,500	105,500
79				97,500	101,50	105,50	101,500	105,500
80				98,000	102,00	105,50	105,500	105,500
81				98,500	102,50	105,50	105,500	105,500
82				99,000	103,00	105,50	105,500	105,500
83				100,000	104,00	105,50	105,500	105,500
84					104,50	105,50	105,500	105,500
85					105,00	105,50	105,500	105,500
86+					105,50	105,50	105,500	105,500

Some states maximum gross weight limits may be less than or greater than 105,500 pounds. See appendix in the back of the book for each states maximum gross weight limits with and without permits.

- 3.01 Scope:** These guides pertain to routinely permitted nondivisible load vehicles moving over the National Network and other state highways designated by the individual states. It is intended that the guidelines provided herein will establish a basis for WASHTO interstate permit agreements.
- 3.02 Maximum Weights:**
- 3.02.1 Pounds Per Inch of Tire Width:** The maximum load on any tire on a vehicle or vehicle combination shall be 600 lb/in.
- 3.02.2 Single Axle:** The maximum load on a single axle equipped with dual tires (four tires/axle) shall be 24,000 pounds.
- 3.02.3 Tandem Axles:** The maximum load on a tandem axle (four tires/axle) shall be 45,000 pounds.
- 3.02.4 Tridem and Trunnion Axles:** When used in interstate truck transportation, the maximum load on either tridem or trunnion axle shall not exceed 60,000 pounds for routine permitting. Maximum loads above 60,000 pounds would be subject to each states permitting requirements or analysis.
- 3.02.5 Manufacturer's Ratings:** Where the manufacturer's ratings for tires, axles, or axle groups is less than the maximum weights recommended in this section, the manufacturer's rating will govern.
- 3.03 Maximum Dimensions:**
- 3.03.1 Width:** Maximum width of the vehicle or combination of vehicles, including load, shall not exceed 10 feet.
- 3.03.2 Height:** Maximum height of the vehicle or combination of vehicles, including load, shall not exceed 14 feet.
- 3.03.3 Length:** Maximum length of the vehicle or combination of vehicles, including load, shall not exceed 90 feet.
- 3.04 Time of Movement:** Oversize/overweight vehicles, which meet the requirements of this chapter, shall not be subject to time-of-day, weekend, or holiday movement restrictions, except for special circumstances dictated by each state.

- 3.05 Route Restrictions:** Oversize vehicles, which meet the requirements of this chapter, shall be restricted to highways with four or more traffic lanes and shall be further restricted to use of service facilities and commercial areas immediately adjacent to those highways.
- 3.06 Pilot/Escort Vehicles:** Oversize/overweight vehicles, which meet the requirements of this chapter, shall not require pilot/escorts except for special circumstances dictated by each state.
- 3.07 Lighting, Flagging, and Signing:** Oversize vehicles shall be equipped with flags, signs, and lights in accordance with [Chapter 7.00](#).

4.01 Scope: The purpose of this chapter is to describe the basic common procedures used to issue permits for movement of certain nondivisible oversize and/or overweight vehicles on highways designated by the jurisdictions participating in the “Western Regional Agreement.”

4.02 Envelope Vehicle: Each member jurisdiction may issue regional permits allowing operations in any other member jurisdiction of the following nonreducible maximum dimensions:

4.02.1 Weight:

1. 600 pounds per inch of tire width.
2. 21,500 pounds per axle.
3. 43,000 pounds per tandem axle.
4. 53,000 pounds per tridem (wheelbase more than 8 feet and less than 13 feet).
5. 160,000 pounds gross weight.
6. In no case may the gross weight exceed the sum of the permitted axle, and/or axle group weights or the weight specified by the permit, whichever is less.
7. A minimum of five axles are required when hauling (i.e., tractor/semitrailer combination). Self-propelled or towed vehicles may have less than five axles.
8. The weight on any group of axles shall be determined by the weight table on [pages 19 and 20](#). Other provisions of this section shall also apply; the lesser of these weights shall be the determining factor.

4.02.2 Length: 110 feet overall. The Agreement does not authorize permits for a semitrailer longer than 53 feet to carry more than one item, or for any unladen semitrailer longer than 53 feet used in a truck-tractor and semitrailer combination.

Movement of unladen vehicles must comply with the limitations of the jurisdiction being traveled through (i.e., loading jeep and/or booster onto trailer when semitrailer exceeds 62 feet in Oregon).

Note: To avoid unnecessary enforcement action in certain jurisdictions, both **overall length** and **trailer length** must be included on the permit.

4.02.3 Width: 14 feet

4.02.4 Height: 14 feet

- 4.03 Jurisdiction of Issuance:** For vehicles falling within the size and weight limitations as described in the envelope vehicle definition, Member Jurisdictions may issue permits and collect fees for all Member Jurisdictions when they are either the entry/origin, destination/exit, or a pass through jurisdiction on the route. This restriction is designed to allocate the burden of permit writing among the member jurisdictions.

Regardless of which Member Jurisdiction writes the permit, the permit must be acquired prior to any significant movement within any of the Member jurisdictions.

- 4.04 Jurisdiction Contacts:** A list of the contact person for each member jurisdiction is included in the Agreement.

- 4.05 Routes – Regional Highway Network:** The Member Jurisdictions jointly develop and maintain a regional highway network map. Each regional permit may only authorize a permittee to travel on routes designated as part of the regional highway network, with the following exceptions:

1. The issuing jurisdiction may issue a regional permit authorizing travel off of the regional highway network within the borders of the issuing jurisdiction.
2. The issuing jurisdiction may issue a regional permit authorizing travel off the regional highway network when authorization is obtained from the affected jurisdiction.

- 4.05.1 Current Map Maintained by a Designated Jurisdiction:**
A single state shall be designated to maintain the current regional highway network map. Map revisions are to be provided to that state representative.

- 4.05.2 Regional Highway Network Modifications:** Each Member Jurisdiction is responsible to notify the designated state of all restrictions on the network, such as detours due to construction or spring load limits. Notification of changes shall be made at least seven days prior to the effective date of the proposed change, unless an emergency occurs which would not allow advance notice. The designated state will maintain the list of restrictions for each state and distribute copies to each member. Updates are sent out within three days of when the designated state is notified of a change.

In the event of an emergency change in the network, the Member Jurisdictions should make reasonable efforts to notify all other Member Jurisdictions by telephone or FAX of the change and immediately follow up with written notification.

4.05.3 Routes Covered: The Agreement applies only to the highways of the Regional Network. When a permittee must use highways off the Regional Network (i.e., local jurisdiction), the permittee must contact the non-member jurisdiction to obtain any necessary permit. Member jurisdictions must make a notation on any regional permits issued when the permittee must contact a non-member jurisdiction for route approval.

4.06 Special Conditions: Each jurisdiction involved in the Agreement has developed special conditions for travel. The conditions are attached to the permit and include such restrictions as special signing, lighting, escort requirements, or restrictions on days or times of travel.

4.06.1 All Current Special Conditions Maintained by Designated Jurisdiction: A single state shall be designated to maintain the list of special conditions for each state and distribute copies to each member. Updates are sent out within three days of when the designated state is notified of a change.

4.06.2 Modifications: Any changes to a member's special conditions must be conveyed to the designated state 14 days prior to the effective date. In the event of an emergency change in conditions the member making the change should notify the other members by FAX and follow up with written notification.

4.07 Application and Permit Form: A sample permit form is included on [pages 21 and 22](#). This single trip permit will be valid for a period of five working days, for use by the permitted vehicle only (tractor and trailer).

4.08 Period of Permit Validity, Extensions, and Amendments:

4.08.1 Permits may be issued not more than five calendar days in advance of the effective date to assist permittee scheduling and permit offices in leveling work load. Additional advance issuance could jeopardize safety due to the changing status of the system, such as detours, maintenance activities, etc.

4.08.2 One extension of up to five days may be granted upon a showing by the permittee that weather or road conditions or a mechanical failure of the vehicle precluded completion of the permitted trip during the initial five-day period.

4.08.3 An amendment may be allowed for vehicle information, weight and/or dimension due to a mechanical failure and resulting equipment transfer while in route. Routes may also be amended due to an emergent restriction put in place while in route.

4.08.4 An extension or amendment should be made by the issuing jurisdiction, unless either of these only affect your jurisdiction. A hard copy of the extension and/or amendments must accompany the original permit for review by other affected jurisdictions. Hand written corrections on the original permit are not acceptable unless they affect only your jurisdiction.

A jurisdiction may charge for amendments to permits.

4.09 Permitting Process:

4.09.1 Overview: When all of the procedures required to implement this program are in place, the following briefly describes how the process should work:

1. The truck industry, permit service companies and all interested parties will be given copies of the regional highway network map and a summary of the requirements for operating envelope vehicles under the Regional Agreement (as contained in this document). Each truck driver using these procedures is strongly encouraged to carry a copy of this map on board the permitted vehicle.
2. Each jurisdiction will be responsible for publicizing the regional permitting program and related procedures.
3. The truck operator requiring a regional permit for a nondivisible overweight or oversize vehicle that falls within the limits of the Agreement will contact the issuing jurisdiction and request a permit for a specific trip (with a specific origin and destination) and for a specific vehicle.
4. If the vehicle and the highway route required to make the trip falls entirely within the limits of the Agreement, the issuing jurisdiction will:
 - a. Issue the regional permit.
 - b. Collect the fees; the sum of all the individual fees of each jurisdiction through which the envelope vehicle will travel.
 - c. Designate the specific routes that can be used.

5. If the issuing jurisdiction is issuing a permit that terminates in another member jurisdiction, and the permittee is going to leave the designated system, **the issuing jurisdiction must contact the member jurisdiction** in which the movement is exiting the regional network. They must give permission to exit the network before the permit is to be issued. The name of the contact person should be included on the permit
6. If one (or more) of the requirements of this common procedure are not met for one or more jurisdictions, then the issuing jurisdiction will require the truck operator to obtain individual state permits in those states where the requirements cannot be met.
7. After receiving the regional permit, the truck operator must also comply with all the special conditions attached to the permit.
8. Each jurisdiction will be responsible for the enforcement of the nondivisible oversize and overweight laws within its own boundaries. The regional permit will be valid in each jurisdiction for which it has been issued. However, if the requirements of the permit have been violated in a particular jurisdiction, the individual requirements of that jurisdiction will be enforced. Any fines collected within that jurisdiction will continue to be retained in that jurisdiction with no reimbursement being made to any other jurisdiction (including the issuing jurisdiction).

4.09.2 Transmittal of Fees:

1. A log and/or copies of regional permits must be maintained. A summary report, itemizing permits affecting a specific jurisdiction, or copies of the permits, are to accompany the transmittal to each jurisdiction as support for the fees being remitted.
2. The summary report, if used, must contain a list of permit numbers, the name of the carriers, issue date, fee, and type of permit (i.e., oversize, overweight, size and weight, or overlength). This information is used by the state for federal reporting and other statistical purposes.
3. Transmittals are to be prepared monthly with the actual remittance occurring as timely as possible after the completion of each calendar month.

4.09.3 Fee Calculation: Each jurisdiction has developed its own current fee schedule and method for collecting the fees. The most current fees will be maintained in a desk guide for use by each member jurisdiction.

Note: No permit fees will be charged when issuing a Regional Permit to marked vehicles belonging to, and operated by, government entities (i.e., marked military vehicles).

4.10 Vehicle Weight Table:

(See table on the following pages.)

Maximum Load in Pounds Carried on Any Group of 2 or More Consecutive Axles

Distance in Feet Between First and Last Axle of Any Group of Consecutive Axles

	2 Axles	3 Axles	4 Axles	5 Axles	6 Axles	7 Axles	8 Axles	9 Axles	10 Axles
4	43000								
5	43000								
6	43000								
7	43000								
8	43000	53000							
9	43000	53000							
10	43000	53000							
11		53000							
12		53000	70000						
13		53000	70900						
14		64500	71900						
15			72800						
16			73700	81200					
17			74700	82100					
18			75600	83000					
19			76500	83300					
20			77500	87400					
21			78400	85600					
22			79300	86500					
23			80300	87300					
24			81200	88200					
25			82100	89100					
26			83100	90000					
27			84000	90800					
28			84900	97200	99100				
29			85900	92600	100000				
30			86000	93400	100800				
31				94300	101600				
32				95200	102500				
33				96100	103300				
34				97000	104200				
35				97800	105000				
36				98700	105800				
37				99600	106700				
38				100500	107500				
39				101300	108400				
40				102200	109200				
41				103100	110000				
42				104000	110900				
43				104800	111700				
44				105700	112600				
45				106600	113400				
46				107500	114200	121600			
47					115100	122400			
48					115900	123200			
49					116800	124000			

	2 Axles	3 Axles	4 Axles	5 Axles	6 Axles	7 Axles	8 Axles	9 Axles	10 Axles
50					117600	124800			
51					118400	125700			
52					119300	126500			
53					120100	127300			
54					121000	128100	135600	143300	151200
55					121800	128900	136400	144100	152000
56					122600	129700	137200	144900	152800
57					123500	130600	138000	145700	153500
58					124300	131400	138800	146500	154300
59					125200	132200	139600	147300	155100
60					126000	133000	140400	148100	155900
61					126800	133800	141200	148800	156600
62					127700	134600	142000	149600	157400
63					128500	135500	142800	150400	158200
64					129000	136300	143600	151200	159000
65						137100	144400	152000	159800
66						137900	145200	152800	160000
67						138700	146000	153600	
68						139500	146800	154400	
69						140400	147600	155100	
70						141200	148400	155900	
71						142000	149200	156700	
72						142800	150000	157500	
73						143600	150800	158300	
74						144400	151600	159100	
75						145300	152400	159900	
76						146100	153200	160000	
77						146900	154000		
78						147700	154800		
79						148500	155600		
80						149300	156400		
81						150200	157200		
82						150500	158000		
83							158800		
84							159600		
85							160000		

Note: In Oklahoma, a 9 or 10 axle configuration is limited to the allowances under the 8 axle configuration column.

4.11 Sample Western Regional Permit:

<u>WESTERN REGIONAL PERMIT</u>						State Logo																																														
Permit Number:		Start:		Expires:																																																
Location:		Agent Name:																																																		
Company Name:																																																				
Address:																																																				
Vehicle/Axle:	Truck -	Trailer-	Tractor -	Semi-Trailer -	Self Prop. -	Other -																																														
License:	VIN:		Unit:		Base State:																																															
Transporting(Item):																																																				
Origin:				Destination:																																																
Routing:																																																				
 Restriction:																																																				
<table style="width: 100%; border-collapse: collapse;"> <thead> <tr> <th style="text-align: left;"><u>Weight</u></th> <th style="text-align: center;"><u>(LB)</u></th> <th style="text-align: center;"><u>(KG)</u></th> <th style="text-align: left;"><u>Dimensions</u></th> <th style="text-align: center;"><u>(FT - In)</u></th> <th style="text-align: center;"><u>(Meters)</u></th> <th style="text-align: left;"><u>Dimensions</u></th> <th style="text-align: center;"><u>(FT - In)</u></th> <th style="text-align: center;"><u>(Meters)</u></th> </tr> </thead> <tbody> <tr> <td>Gross Vehicle WT</td> <td></td> <td></td> <td>Max Height</td> <td></td> <td></td> <td>Front overhang</td> <td></td> <td></td> </tr> <tr> <td>Gross Axle WT</td> <td></td> <td></td> <td>Max Width</td> <td></td> <td></td> <td>Rear overhang</td> <td></td> <td></td> </tr> <tr> <td>Legal Axle WT</td> <td></td> <td></td> <td>Total Length</td> <td></td> <td></td> <td></td> <td></td> <td></td> </tr> <tr> <td>Excess Axle WT</td> <td></td> <td></td> <td>Trailer Length</td> <td></td> <td></td> <td></td> <td></td> <td></td> </tr> </tbody> </table>								<u>Weight</u>	<u>(LB)</u>	<u>(KG)</u>	<u>Dimensions</u>	<u>(FT - In)</u>	<u>(Meters)</u>	<u>Dimensions</u>	<u>(FT - In)</u>	<u>(Meters)</u>	Gross Vehicle WT			Max Height			Front overhang			Gross Axle WT			Max Width			Rear overhang			Legal Axle WT			Total Length						Excess Axle WT			Trailer Length					
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(SPECIAL CONDITIONS)				Total Fee:																																																
Not Valid without Applicable State Conditions Attached - Sign in colored ink (not black) to validate permit. Carry fax permit in vehicle all times.																																																				
READ THIS BEFORE SIGNING:																																																				
Valid on State Highways ONLY. Other routes subject to local road authority. Weight and speed restrictions subject to spring breakup. Permittee is responsible for vertical clearances. Applicant's acceptance of permit certifies vehicle-load has been reduced to minimum practical size - weight and is an agreement to comply with all applicable State permit regulations and all applicable State or Federal Motor vehicle laws. By signing, the permittee																																																				
SIGNED X _____																																																				
PERMIT NOT VALID UNLESS SIGNED BY PERMITEE AGENT THEREOF																																																				

WESTERN REGIONAL PERMIT

Permit Number:

Starts :

Expires:

Attachment**Axle Spacing Information**

Axle Group #

Axles

Group Wgt(lb)

Tires

Axle Space

Tire Size

Ft

In

5.01 Scope: Superloads by definition are moves that require special approval. This chapter identifies recommended procedures for the approval process for superloads, specific size and weight criteria for each state's superload determination, and a sample form that may be used to notify other states of proposed moves.

5.02 Superload Permit Application:

5.02.1 The following information, at a minimum, should accompany the standard permit application:

1. Documentation that the move is in the public interest and that an alternative method of transport is not feasible.
2. A schematic or photograph of the item to be moved, including an explanation of why it cannot be moved in smaller pieces.
3. A schematic of the loaded vehicle(s), including axle loadings, axle spacings (measured from the center of each axle), tire sizes, number of tires per axle, and the proposed height, length and width of the configuration.
4. A traffic control plan depicting the route and specific procedures to be followed to provide safe movement along the route, including:
 - a. Identified locations where anticipated traffic delays will occur.
 - b. Description of any lane restrictions.
 - c. How pilot/escort vehicles and flag persons will be used.
 - d. Arrangements for the movement of overhead obstacles.
 - e. Identification of railroad crossings and contact information, including a pre-trip analysis of each crossing to assure vehicle(s) will clear the grade.
 - f. Provisions for emergency vehicles to navigate around the configuration.

5.02.2 Each state should determine the most efficient means of analyzing superload requests and processing superload permits without jeopardizing safety or infrastructure preservation. Analysis and processing time for superloads should be set in policy and shared with industry.

5.03 State Definition of Size and Weight Criteria for Superloads:

See map and notes on [pages 26 and 27](#).

5.04 Superload Alert Process:

5.04.1 Using the criteria in the WASHTO Guide on [pages 26 and 27](#), the WASHTO state receiving an inquiry or request for a permit, which exceeds maximum routine permit dimensions, should use the following procedure to assure that the appropriate states are notified of a possible “superload” move.

1. If the movement is a “superload” in one or more states, complete the superload alert fax transmittal sheet.
2. Advise the carrier that dimensions and/or weights require special handling by one or more of the states. The carrier should be advised to contact each state. Provide a contact name, phone number, and fax number for each state.
3. Send the completed fax transmittal sheet to each state involved in the move.

5.04.2 WASHTO Border states may wish to add their neighboring state even if that state is not a WASHTO member.

5.04.3 The following is a list of phone numbers and fax numbers for the WASHTO States Permit Offices.

Permit Offices	Phone Number	Fax Number
Alaska, Anchorage	(907) 365-1200	(907) 365-1221
Arizona, Phoenix	(623) 932-2247	(623) 932-2441
California, Sacramento	(916) 654-4828	(916) 654-3498
Colorado, Denver	(303) 757-9539	(303) 757-9719
Hawaii	(808) 831-6712	(808) 831-6725
Idaho, Boise	(208) 334-8420	(208) 334-8419
Montana, Helena	(406) 444-6130	(406) 444-7670
Nevada, Carson City	(775) 888-7410	(775) 888-7103
New Mexico, Santa Fe	(505) 827-0376	(505) 827-0384
North Dakota, Bismarck	(701) 328-2621	(701) 328-1642
Oklahoma, Oklahoma City	(405) 425-2334	(405) 425-0943
Oregon, Salem	(503) 373-0000	(503) 378-2873
South Dakota, Sisseton	(605) 698-3925	(605) 698-7665
Texas, Austin	(800) 299-1700	(512) 465-3549
Utah, Salt Lake City	(801) 965-4892	(801) 965-4936
Washington, Olympia	(360) 704-6390	(360) 704-6350
Wyoming, Cheyenne	(307) 777-4376	(307) 777-4399

5.04.4 Superload Information Transmittal Sheet:

To:	Fax Number	
AK	(907) 365-1221	From: _____
AZ	(623) 932-2411	
CA	(916) 654-3498	Date: ____ / ____ / ____
CO	(303) 757-9719	
ID	(208) 334-8419	Company Name: _____
MT	(406) 444-7670	
NV	(775) 888-7103	Contract: _____
NM	(505) 827-0384	
ND	(701) 328-1642	Load: _____
OK	(405) 425-0943	
OR	(503) 378-2873	Dimensions: Width: _____
SD	(605) 698-7665	Length: _____
TX	(512) 465-3549	
UT	(801) 965-4936	No. Axles: _____
WA	(360) 704-6350	
WY	(307) 777-4399	Axle Spacings: _____

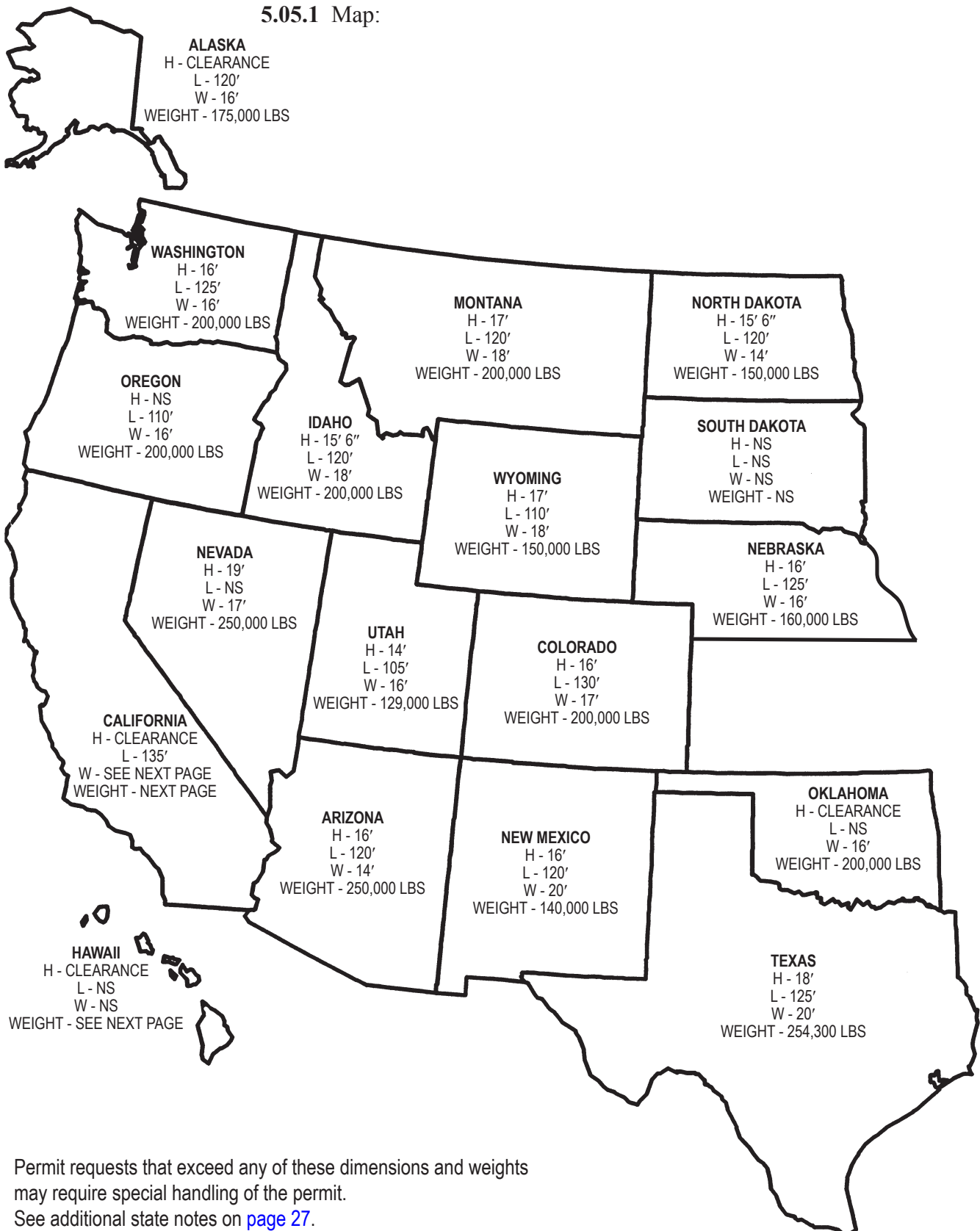
		Axle Weights: _____

		Route: _____

Other Information: _____

5.05 Superloads Defined by State:

5.05.1 Map:



5.05.2 Additional State Notes:

California: Special handling for width over 15 feet.

Maximum weights allowed on “purple chart” may not be exceeded. Additional axles must be added or weight of the load reduced to conform to “purple chart loading.” No exceptions are made. Acceptable hauling equipment should be used.

Hawaii: Special handling and review by engineering is required if the following is exceeded:

Tread width of 8 feet, over 42,000 on a single axle.

Tread width of 8 feet, over 56,000 on a tandem.

Tread width of 9 feet, over 45,000 on a single axle.

Tread width of 9 feet, over 60,000 on a tandem.

Tread width of 10 feet, over 48,000 on a single axle.

Tread width of 10 feet, over 64,000 on a tandem.

Washington: Special handling for height. It is the responsibility of the permittee to carefully plan the route. Some structures on state highways do not have a 14-foot clearance; many have less than 14 feet 6 inches. All movements in excess of 14 feet 6 inches high shall be preceded by an escort vehicle with height pole.

The superload length of 125 feet is trailer/load length, exclusive of the pulling power unit.

Oklahoma: Weight requiring independent route review is dependent upon the configuration of the vehicle(s) and specific route.

- 6.01 Scope:** The provisions of this chapter apply to allowed dimensions of manufactured housing and to minimum requirements for the tow vehicles used for transport.

It is further intended that the guidelines provided in this section will form the basis for WASHTO interstate permitting agreements for manufactured housing.

6.02 Manufactured Housing Recommended for Interstate Permit Agreements With a Base of 14 Feet Wide or Less:

6.02.1 Width: Maximum width of 14 feet plus 2 feet allowed for eaves.

6.02.2 Length:

1. Maximum length for a housing unit, including tongue – 80 feet.
2. Maximum combined length for housing unit and tow vehicle – 90 feet.

6.02.3 Height: Maximum height of housing unit in transport – 14 feet – limited to the vertical clearance of the route.

6.02.4 Axles: Axles must be in sufficient number to support enough tires to comply with the tire loading requirements in this section.

6.02.5 Brakes: Brakes must be designed and installed to activate if the housing unit accidentally breaks away from the towing vehicle. Brakes must operational on all wheels.

6.02.6 Tire Loadings: Tire loadings are dependent on when the housing unit was manufactured and must comply as follows:

1. **Tire loadings** on housing units manufactured **after January 1, 2002** (labeled pursuant to 24 CFR 3282.362 (c) (2)(i)) may not exceed the manufacturer's rating as marked on the sidewall. In the absence of a sidewall marking, the tires on the housing unit must comply with the load rating specified in any of the publications of any organization listed in the Federal Motor Carrier Safety Standard (FMCSS) No. 119 (49 CFR 571.119, S5.1 (b)). Housing units with no verifiable date of manufacture must also not exceed the manufacturer's tire load rating.

2. **Tire loadings** on housing units manufactured **before January 1, 2002** (labeled pursuant to 24 CFR 3282.362 (c)(2)(i)) must not exceed more than 18 percent above the manufacturer's rating as marked on the sidewall. In the absence of a sidewall marking, the tires on the housing unit must not exceed 18 percent above the load rating specified in any of the publications of any organization listed in the Federal Motor Carrier Safety Standard (FMCSS) No. 119 (49 CFR 571.119, S5.1 (b)). Housing units transported on tires overloaded by 9 percent or more must not be moved at speeds exceeding 50 miles per hour (50 mph or 80 km/hr).

6.03 Manufactured Housing With a Base in Excess of 14 Feet Wide:

Housing units in excess of the dimensional criteria recommended for interstate permitting agreements are subject to the criteria of the individual states.

6.04 Tow Vehicles Requirements:

6.04.1 Equipped with at least one drive axle with dual wheels.

6.04.2 Wheelbase of at least 100 inches.

6.04.3 Have a minimum combined gross axle weight rating, assigned by the manufacturer, to meet the requirements of the tow vehicle plus housing unit.

6.04.3 Have sufficient engine horsepower to maintain posted speed limits or trucks on the interstate, with the acknowledgment that each state retains the right to regulate the speed of a transported manufactured housing unit.

6.05 Time of Travel: Movement shall be allowed in accordance with the provisions of [Chapter 7.00](#).

6.06 Lights, Signs, and Pilot/Escorts: Lights, signs, and the use of pilot/escort vehicles shall meet the provisions of [Chapters 7.00](#) and [8.00](#).

7.01 Scope: The provisions of this chapter are intended to supplement legal requirements for motor vehicles and apply to vehicles transporting oversize nondivisible loads, including manufactured housing.

7.02 Lights, Flags, and Signs:

7.02.1 Lighting Requirements for Oversize Loads Traveling After Dark: Those overdimensional loads traveling during hours of darkness shall be required to display lights to mark the extremities of the vehicle and/or load, and shall be, in addition to those clearance lights, required on legal size vehicles when traveling at night.

1. Standards for Lights on Oversize Vehicles and/or Loads

- a. Lights are only required on those vehicles traveling after dark. (Dark is defined as 1/2 hour after sunset to 1/2 hour before sunrise.)
- b. The lights must be visible from a minimum of 500 feet.
- c. The lights shall be steady burning.
- d. The color of the lights shall be as follows:
 - (1) Lights visible from the front of the vehicle and/or load and the extremities in the middle or near the front of the vehicle and/or load shall be amber.
 - (2) Lights visible from the back of the vehicle and/or load and the extremities near the back of the vehicle and/or load shall be red.

2. Standards for Lights on Rear Overhang

- a. Lights are required when rear overhang exceeds the end of the trailer by 4 feet or more.
 - (1) If the overhang is 2 feet wide or less, only one light is required on the end of the load.
 - (2) If the overhang is over 2 feet wide, two lights are required on the end of the load to show the maximum width of the overhang. (See diagrams at end of chapter.)

3. Standards for Warning Lights on Towing Vehicles

- a. Amber flashing lights displayed on a towing vehicle shall be mounted above the cab and meet the following:
 - (1) Rotating or flashing beacon visible from a minimum of 500 feet.

7.02.2 Flagging Requirements for Oversize Loads:

1. Warning Flags: All overwidth and overlength vehicles and/or loads, excluding longer combination vehicles, shall be marked by warning flags meeting the following:
 - a. Size – 18 inches by 18 inches.
 - b. Color – Solid Red or Fluorescent Orange.
2. Placement of Flags
 - a. On overwidth loads, flags shall be placed at the four corners and/or extremities of the vehicle and/or load.

Front – Fastened to each front corner of the vehicle and/or load if it exceeds legal width.

Rear – Fastened to each corner of the extremities of the vehicle and/or load at the rearmost part of the vehicle or load if it exceeds legal width.

Side – Fastened to mark any extremity of size if wider than the front or the rear (see examples).
 - b. If rear overhang exceeds the end of the trailer by 4 feet or more, one flag is required at the extreme rear, if the width of the projection is 2 feet wide or less. If the projection exceeds 2 feet in width, then two flags are required at the extreme rear to indicate the maximum width. (See diagrams at end of chapter.)

7.02.3 Signing Requirements for Oversize Loads: “OVERSIZE LOAD” signs shall meet the following specifications:

1. Dimensions: 18 inches high by 7 feet wide, letter height 10 inches, letter type Standard Series C, stroke width 15/8 inch, black letters on yellow background. When it is impractical to mount a sign of the preferred dimension (e.g., on the front of the power unit) a sign not less than 10 inches high by 5 feet long with 8 inch letters in a 1 inch brush stroke is authorized.
2. Displaying Signs: “OVERSIZE LOAD” signs shall be displayed on the front or roofline of the towing vehicle and the rear of the oversize load.

3. When Signs Are Required: "OVERSIZE LOAD" signs shall be required on all vehicles and/or loads exceeding legal width. Signs shall not be displayed when the vehicle is empty and of legal dimensions.

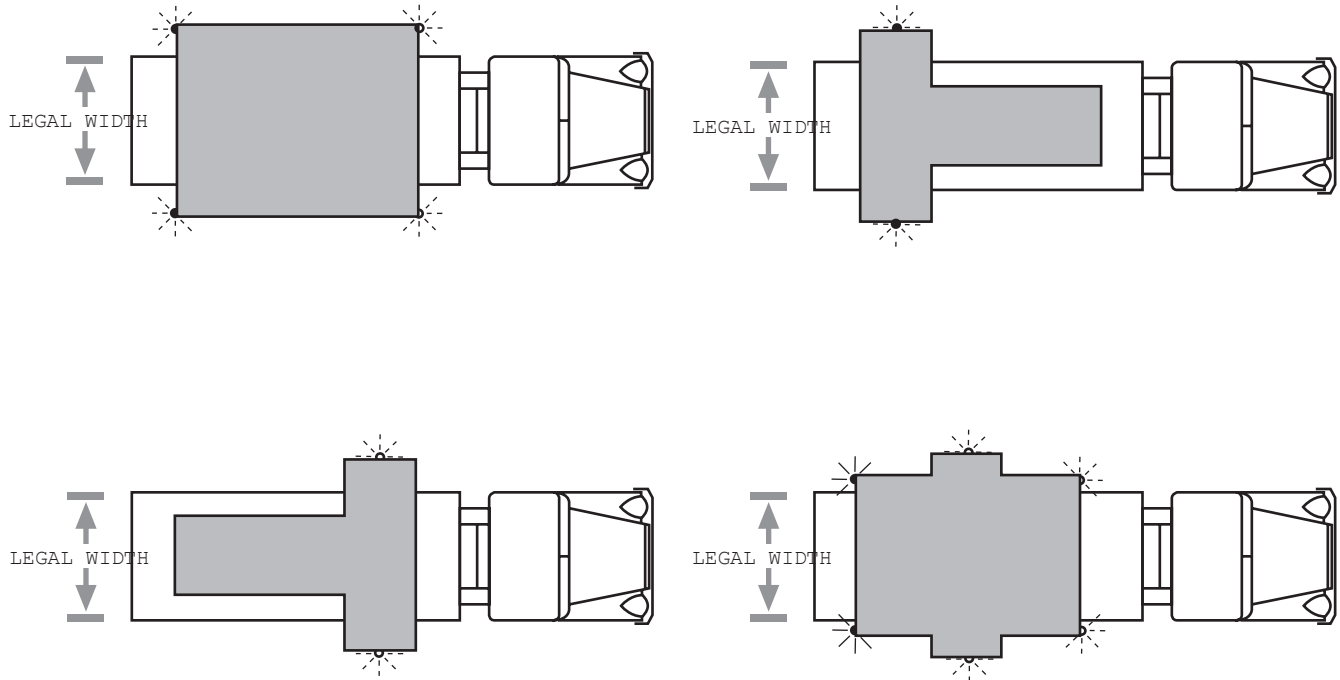
7.03 Conditions Requiring Escorts: Escorts will be required for extra-legal vehicle moves in the following listed conditions:

1. On two-lane, two-way roadways, one escort in advance of the extra-legal vehicle when:
 - The load exceeds 12 feet in width, but is 14 feet or less in width.
 - The load exceeds 105 feet in length.
2. On four-lane and divided roadways, one escort will be required to the rear of the extra-legal vehicle when:
 - The load exceeds 14 feet in width.
 - The load exceeds 120 feet in length.
3. On all roadways, a front or rear escort will be required when an overhang exceeds 20 feet:
 - If the overhang is to the front of the vehicle, the escort will position to the front.
 - If the overhang is to the rear of the vehicle, the escort will be positioned to the rear.
 - In the event there is an overhang exceeding 20 feet at both the front and rear of the vehicle, there will be one escort positioned to the front and one escort positioned to the rear.
4. On two-lane, two-way roadways, two escorts, one in advance and one following the extra-legal vehicle, are required when:
 - The load exceeds 14 feet in width, but is 16 feet or less in width.

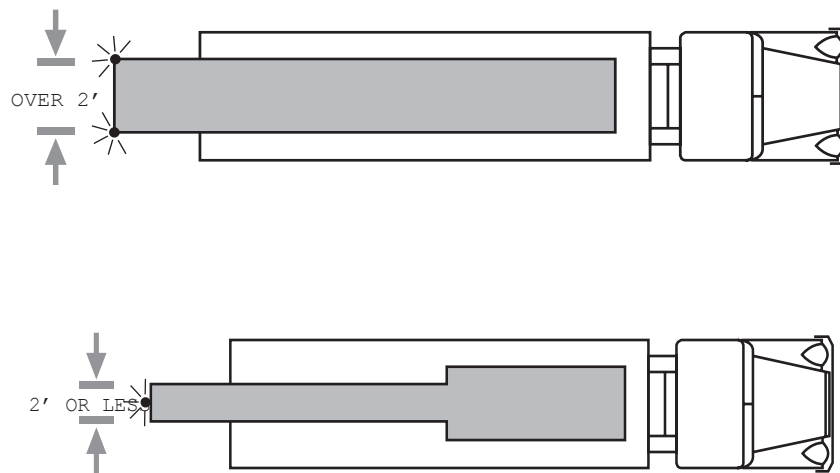
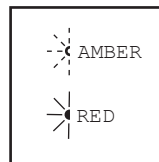
7.04 Time of Movement:

1. Movement of any oversize vehicle or load shall be allowed any day during daylight hours (1/2 hour before sunrise to 1/2 hour after sunset) except during holiday restrictions.
2. States may allow nighttime movement depending on traffic conditions. Load movements other than daylight hours will be noted on the permit.
3. Holiday restrictions: Movement of any oversize load shall be prohibited the day before the holiday, the holiday, and the day after the holiday for those national holidays listed below:

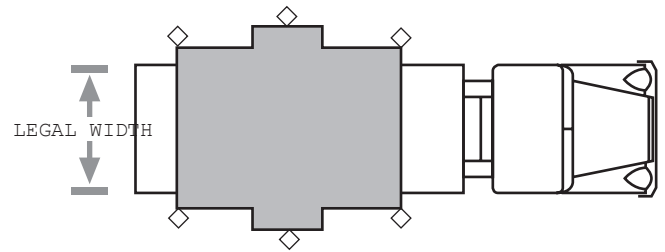
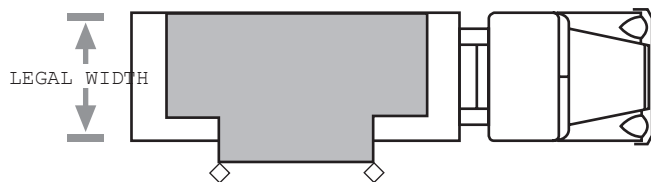
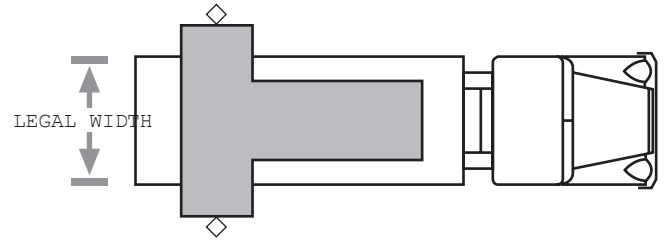
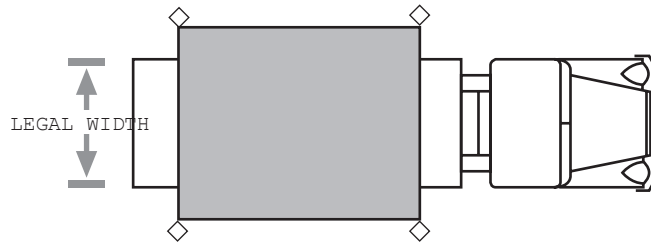
New Years Day	Labor Day
Memorial Day	Thanksgiving Day
Independence Day	Christmas Day



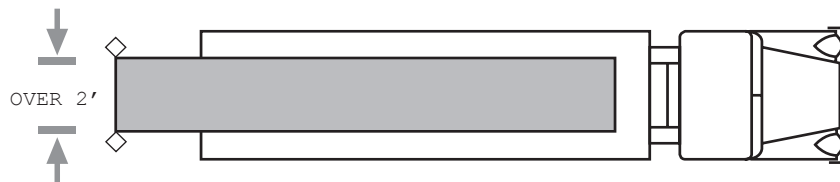
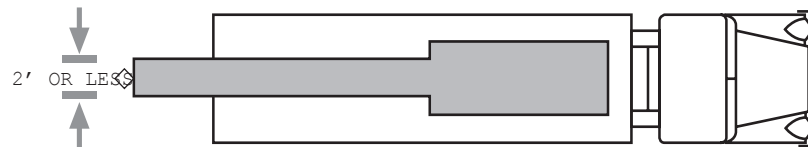
Examples for Warning Lights On Overwidth Loads



**Examples for Red Warning Lights On Overlength Loads With a
Rear End Overhang of Four Feet or More From End of Trailer**



Red or Florescent Orange Warning Flags on Overwidth Loads



**Or Loads With a Rear End Overhang of 4 Feet
or More From End of Trailer**

- 8.01 Scope:** The contents of this chapter include the Pilot Car Escort Best Practices Guidelines and are designed to give any jurisdiction the tools necessary to regulate the use of pilot/escort vehicles. Included are recommended minimum requirements for pilot/escort vehicle operators and equipment.

This chapter will be updated as needed using the most current version of the Best Practices Guidelines accessible via the Federal Highway Administration (FHWA) Web site at: http://ops.fhwa.dot.gov/freight/documents/pilotcar_bpguideline/pilot_car.pdf.

8.02 Pilot/Escort Vehicle Operator Certification:

8.02.1 The operator of a pilot/escort vehicle must obtain and maintain certification as provided in this chapter to be authorized to escort an extra-legal vehicle or load on a state highway. The permittee of the extra-legal vehicle or load shall be responsible to inspect a pilot/escort vehicle operator's certification to ensure that it is valid and current. To obtain a certification, the pilot/escort operator must:

1. Submit an application for certification to the appropriate regulatory agency, in the form required by said agency. The application must contain all required information and documentation.
2. Receive a minimum of 8 hours of training specific to pilot/escort requirements and best practices, including principals of defensive driving.
3. Possess a valid motor vehicle driver's license.
4. Possess the minimum insurance as required by the state(s) where the escort vehicle will be operated.

8.02.2 The pilot/escort vehicle certification examination will consist of pilot/escort vehicle operation requirements and best practices, including:

1. Pre-trip planning; pre-trip meetings, oversize/overweight load movement and post trip analysis and evaluation; and
2. Requirements of applicable traffic laws; and
3. Procedures for operator flagging and traffic control, as described in the 2003 *Manual on Uniform Traffic Control Devices* (MUTCD), Part 6, Section E, or as may subsequently be revised.

8.02.3 The certifying regulatory agency may set a fee to cover administrative costs.

8.02.4 The regulatory agency will grant a certificate to all persons who pass the pilot/escort vehicle certification examination and satisfy the other requirements of Section 8.02.1 above.

The certification must be in the possession of the pilot/escort vehicle operator at all times when the operator is escorting an extra-legal vehicle or load on a state highway. The certification shall be exclusive to the operator named in the certificate and may not be assigned or transferred.

8.02.5 The term of the certification shall be for a maximum period of four years, subject to Section 8.02.6 below, and shall expire automatically four years after the date of issuance by the department. Operators must notify the department in writing within 30 days of any change of address or name. Upon expiration of the certification, an operator must again comply with the requirements of Section 8.02.1 before the department will issue a new certificate.

8.02.6 The certification shall be deemed automatically canceled, suspended, or revoked for:

1. Failure to satisfy the requirements of Section 8.02.1 above, or failure to give the required or correct information on the application for certification as a pilot/escort vehicle operator, or the commission of any fraud in making the application.
2. Violation of rules established by the certifying state.
3. Failure to notify the regulatory agency of any cancellation, suspension, or revocation of the operator's motor vehicle driver's license by the "DMV" within five working days of the operator's receipt of notice of the same.
4. Conduct while operating a motor vehicle or a pilot/escort vehicle, which, as determined by the regulatory agency, threatens the public safety or welfare of highway users or creates unreasonable interference with the efficient movement of traffic.
5. Allowing or permitting an uncertified person to operate a pilot/escort vehicle under the operator's supervision or direction.

8.03 Pilot/Escort Vehicle Operations – Best Practices Guidelines:

8.03.1 Pre-trip Planning

1. Confirm Assignment

- a. Carrier Confirmation: Identify load size – width, height, length, and gross weight; identify axle configuration and steering limitations; determine carrier's preferred route or state(s) permit designated route; determine if load is top heavy/over height and if top mounted skid boards and/or "high route survey" will be necessary; identify driver's name, truck number, driver's cell phone number, CB channel of preference and other identifiers and contact numbers; select a CB channel that maintains awareness of public information; identify name and address for point of origin and destination and cross-reference to map.
- b. Permit Confirmation: Secure copies of the permits, as available; secure copies of related route surveys, as available; identify required number and type of escorts.

2. Review Route

- a. Review route for state prohibitions; size restrictions, on-going construction, dimensional load movement curfews, weight restricted bridges, height restricted overhead limitations, seasonal restrictions; feasibility and safety; identify alternate routes as necessary; complete an initial risk assessment to identify potential hazards and obstructions.
- b. Review route for railroad crossings; Assess rail traffic; review crossing profiles; review crossings for change in ascending and descending slope, length of crossing, number of tracks, road condition on the approach and retreat (i.e., straight, turn, curve) and instructions for advance or emergency notification; identify pertinent railroad emergency telephone numbers; make advance contact with the railroad if in doubt that the load can safely negotiate the crossing or as required by permit or state specific regulations.
- c. Estimate daily travel distance.
- d. Identify approved interim stops.
- e. Identify safe parking and lodging for overnight layovers.

- f. Cross-reference route to travel maps or electronic medium.
 - g. Complete or review route surveys as appropriate.
 - h. Complete route sheets.
- 3. Review Permit
 - a. Verify accuracy of load representation, travel dates and time, point of origin and destination, and route designation.
 - b. Note any route restrictions.
 - c. Note special equipment requirements.
 - d. Identify traversed municipalities.
 - e. Verify municipal permits have been secured.
 - f. Note required law enforcement escort(s), utility company accompaniment, etc.
- 4. Check Vehicle/Equipment
 - a. Comply with all state specific and federal vehicle, equipment and safety requirements/certifications, including a review of the Code of Federal Regulations (CFR) Title 49, Volume 4, Chapter III, Part 393, Section 393.95 for regulation on emergency equipment.
 - b. Comply with all other provisions of the law regarding the use, operation, and licensing of motor vehicles.
 - c. Equip and test all operational and safety equipment and ensure cleanliness and serviceability:
 - (1) Lights: In addition to legal and fully operational headlights and taillights the vehicle shall be equipped with one to three amber flashing lights, mounted above the roof line and visible to 500 feet from front and rear.
 - (2) Signs: An “OVERSIZE LOAD” sign (“WIDE LOAD” and “LONG LOAD” may also be acceptable in some jurisdictions) shall be mounted above the roof line, visible from front and rear during the performance of pilot/escort operations, and shall be a minimum of 5 feet long, 10 inches high with 1 inch brush stroke black letters 8 inches high on a yellow background. Two back up signs should also be available during the trip.
 - (3) Flags (See title 49 CFR, Section 393.95).

- (4) Two-way electronic communications.
 - (5) Protective/Safety Clothing: reflective vest, shirt, or jacket to be worn by the operator while directing traffic. Also recommended is a white or orange hard hat to also be worn when outside of the vehicle.
 - (6) Stop/Slow Paddle: Standard 18 inch hand paddle.
 - (7) Fire extinguisher (See CFR Title 49, Section 393.95).
 - (8) First aid kit.
 - (9) Reflective triangles or cones (See CFR Title 49, Section 393.95).
 - (10) Vertical Clearance Measuring Device/Height Pole: Nonconductive and nondestructive to overhead obstacles.
 - (11) Maps.
 - (12) Spare/Replacement equipment (See CFR Title 49, Section 393.95).
 - (13) Flashlight: Equipped with red cone and include extra batteries.
 - d. Perform Vehicle Maintenance: Check tire condition and pressure and spare tire; check hoses and fluid levels; check horn, windows, odometer, battery, speedometer, and lights.
 - e. Verify required vehicle insurance is current.
 - f. Comply with jurisdictional specific regulations regarding restrictions on vehicle contents and occupants.
5. Check Pilot Car Escort
- a. Comply with state escort age requirements.
 - b. Verify required driver's license is current.
 - c. Verify required training has been completed.
 - d. Review Pilot/Escort Operator Job-Readiness: Operator is healthy; operator is free of fatigue and able to use required equipment; operator must not display badge, shield, emblem or uniform of color or design that may be mistaken for law enforcement badge, emblem or uniform.

6. Prepare Contingency Plans

- a. Review contingency plans with the carrier and en route transfer escorts:
 - (1) Plan for vehicle breakdowns.
 - (2) Plan for emergencies (pilot/escort vehicle must not be operated as a law enforcement or emergency vehicle) such as contact with overhead obstruction, railroad crossing issues or accidents and property damage.
 - (3) Plan for en route transfer of escort responsibilities: Assess the feasibility for overlapping escort services in the event a non-stationary transfer of escort services is necessary; coordinate minimal safety procedures to be completed by two-way electronic communication until a safe stop can be made; identify the first available/authorized safe area to stop following any non-stationary transfer of escort responsibilities; identify and coordinate plan with other responsible entities (law enforcement agencies, utility companies, railroads, etc.).
 - (4) Coordinate and record emergency numbers with carrier.

8.03.2 Pre-trip Meeting

1. Identify Team Members
 - a. Discuss roles and responsibilities of individual team members.
 - b. Review load driver and law enforcement escort's expectations of the escort/pilot car driver.
 - c. Pilot/Escort operators will not perform Tillerman duties while performing escorting duties.
2. Complete a Job Hazard/Safety Analysis
 - a. Identify load specific risks; such as, fire risks; explosive potential; hazardous material, including notifying the Environmental Protection Agency and state agencies as necessary and identifying railroad crossing and tunnel restrictions including corresponding notification requirements; load configuration, including protruding components and weight shift potential; collapsible or fragile loads; time-sensitive or perishable materials.

- b. Ensure appropriate emergency equipment is on-hand and available to all team members.
 - c. Review emergency procedures with team; including, procedures for communicating emergency situations and procedures to abort the transport.
 - d. Review load limitations; including, ground clearance, load height and maneuverability and turning limitations.
 - e. Review contingency plans for emergencies, en route transfer of escort responsibilities and vehicle breakdowns; including, identifying point of transfer for an en route transfer of escort, and identifying the first available safe stopping site for an en route transfer of escort responsibilities.
3. Review Permits
- a. Confirm accuracy of travel dates, travel times, point of origin, destination, truck identification and license number, trailer number, route and load dimensions; including measuring the load to validate permit specifications.
 - b. Determine if a Route Survey was completed; including, review for changes by permit agency and review for previously unidentified route obstructions and potential hazards or safety issues.
 - c. Complete route sheets for permit designated route if not done as part of pre-trip planning; including, a review to ensure distribution of route sheets to team and cross reference route to maps or electronic medium.
 - d. Determine if advance notifications are designated for law enforcement, railroads, toll road stations and utilities; including, confirm the indicated advance notifications have been completed.
4. Communications Review
- a. Ensure all members of the team are equipped with a two-way radio.
 - b. Identify two-way radio channel.
 - c. Run a test on communications equipment and designated channel.
 - d. Familiarize yourself with the load driver's voice.

5. Prepare for Load Movement
 - a. Check and mount “OVERSIZE LOAD” signs, flags and pilot car lights in accordance with regulations.
 - b. Determine the placement of the team vehicles during transport.
 - c. Set vertical clearance measuring device, “height pole,” as necessary; including, setting the height consistent with state regulations and in cooperation with the load driver, and daily verify accuracy of height pole setting by visually comparing the pole setting to the highest point of the load when the escort vehicle is parked parallel to the load (ensure the load and the escort vehicles are on level surfaces when measuring).

8.03.3 Modified Pre-trip Meeting

1. Review Pre-trip Planning contingency plan for en route transfer of escort responsibilities
 - a. An en route non-stationary transfer should be the exception.
 - b. Overlapping escort services is the preferred method for transferring escort responsibilities when an en route transfer is necessary.
2. Complete minimum safety procedures by two-way communication.
 - a. Identify team members.
 - b. Test communication equipment.
 - c. Identify load specific risks and risk control measures.
 - d. Identify emergency situations or previously unidentified risks surfaced to date.
 - e. Verify the existence of the permit and applicable permit restrictions.
 - f. Verify the route sheet matches the designated permit route.
 - g. Review vehicle positioning.
 - h. Identify the next available safe area to stop.
3. Stop at the next available safe pull-off area.
 - a. Complete the remainder of tasks listed for the Pre-trip Meeting for a point of origin or stationary transfer.
 - b. Review any unforeseen emergencies surfaced to date.

8.03.4 Oversize/Overweight Load Movement

1. Comply, in all respects, with the state specific vehicle movement laws.
2. Comply with all permit specified requirements, restrictions and special conditions.
3. Position vehicles in compliance with state regulations.
4. Display flashing lights, signs and flags as required.
5. Maintain sufficient team communication to promote public, team and load safety.
 - a. Avoid unnecessary conversation.
 - b. Keep the load driver informed of your location.
6. Monitor and advise load driver of any malfunction of load lights, tie-down straps and chains, and tire and brake problems.
7. Compare team vehicle speedometer readings as necessary.
 - a. Advise load driver of any potential violations of state restrictions; including, noncompliance with posted speeds limits and/or permit speed restrictions.
8. Follow safe maneuvering techniques.
 - a. Estimate braking distance for load.
 - b. Maintain necessary vehicle spacing; including, adjusting spacing for congested areas; adjusting spacing when approaching low clearance structures, narrow bridges and blind curves; complying with state specific regulations for multi-load movements; and, if vehicle spacing is extended for the presence of hazardous conditions, test the communications quality to verify that extending the spacing has not resulted in ineffective communications and adjust vehicle spacing appropriately to maintain quality communications.
 - c. Advise load driver of impending roadway restrictions or hazards and recommend defensive maneuvers; including, narrow structures and bridges; guard rails; elevated and/or rough grade crossings; impaired clearances; construction activity and road closures; changes in traffic flow, including vehicles passing and changes in surrounding traffic speed and volume; change in road or weather conditions; trees, utility lines and soft shoulders; stalled vehicles, mailboxes, telephone poles, pedestrians, and road signs; when

- potential hazard cannot be adequately assessed by sight, load should be safely parked and warning devices placed, escort driver and load driver should proceed and physically assess hazard; determine if alternate route is necessary, identifying alternate route, surveying alternate route and ensuring appropriate notifications are made regarding alternate route (carrier, permit agency and official).
- d. Be aware of traffic control light timing risks; including, communicating situations where a team member is unable to clear the traffic control light.
9. Identify and communicate emergency situations to team
- a. Initiate emergency procedures for breakdowns.
 - (1) Comply with title 49 CFR, Volume 4, Chapter III, Part 392, Section 392.22, regulation on emergency signals.
 - (2) Pull as far off the roadway as possible.
 - (3) Place warning cones and/or triangles only after putting on safety apparel; including, compliance with title 49 CFR, Section 393.22, and providing smooth transition for surrounding traffic.
 - (4) Turn on vehicle emergency flashers and maintain escort vehicle warning lights.
 - (5) Apply specific emergency procedures for nighttime breakdowns; including, using reflective equipment and clothing, and using flashlights and/or lanterns.
 - (6) Apply state permissible flagging procedures.
 - (7) Communicate emergency situation to law enforcement and authorities as necessary.
 - (8) Resolve emergency situation as quickly as possible: including, not abandoning load or leaving load unprotected, and making every effort to minimize disruption of traffic flow.
 - b. Initiate emergency procedures when vertical measuring device contact is made with an overhead obstruction.
 - (1) Comply with title 49 CFR, Volume 4, Chapter III, Part 392, Section 392.22, regulation on emergency signals.
 - (2) Immediately advise load driver.

- (3) Pull team vehicles as far off the roadway as possible.
 - (4) Advise appropriate law enforcement agencies and utilities.
 - (5) Place warning cones and/or triangles only after putting on safety apparel; including, compliance with title 49 CFR, Section 392.22, and provide smooth transition for surrounding traffic.
 - (6) Turn on vehicle emergency flashers and maintain escort vehicle warning lights.
 - (7) Apply specific emergency procedures for nighttime emergency stops; including, using reflective equipment and clothing, and using flashlights and/or lanterns.
 - (8) Apply state permissible flagging procedures.
 - (9) Resolve emergency situation as quickly as possible: including, not abandoning load or leaving load unprotected, and making every effort to minimize disruption of traffic flow.
- c. Initiate emergency procedures for rail grade crossing obstructions.
- (1) Comply with CFR Title 49, Volume 4, Chapter III, Part 392, Section 392.22, regulation on emergency signals.
 - (2) Pull as far off the tracks and roadway as possible.
 - (3) Advise appropriate law enforcement agencies.
 - (4) Advise railroad; including, using emergency contact numbers posted on or near crossing, and deferring to law enforcement contact in the absence of a railroad emergency contact number.
 - (5) Advise carrier.
 - (6) Place warning cones and/or triangles only after putting on safety apparel; including, compliance with title 49 CFR, Section 392.22, and considering the blind spots and corners when placing warning devices.
 - (7) Turn on vehicle emergency flashers and maintain escort vehicle warning lights.

- (8) Apply specific emergency procedures for nighttime emergency stops; including, using reflective equipment and clothing, and using flashlights and/or lanterns.
 - (9) Apply state permissible flagging procedures.
 - (10) Resolve emergency situation as quickly as possible: including, not abandoning load or leaving load unprotected, and making every effort to minimize disruption of traffic flow.
- d. Initiate emergency procedures for accidents and property damage.
 - (1) Comply with CFR Title 49, Volume 4, Chapter III, Part 392, Section 392.22, regulation on emergency signals.
 - (2) Pull as far off the roadway as possible.
 - (3) Place warning cones and/or triangles only after putting on safety apparel; including compliance with title 49 CFR, Section 392.22, and providing a smooth transition for the surrounding traffic.
 - (4) Turn on vehicle emergency flashers and maintain escort vehicle warning lights.
 - (5) Apply specific emergency procedures for nighttime emergency stops; including, using reflective equipment and clothing, and using flashlights and/or lanterns. Contact law enforcement, emergency services as necessary.
 - (6) Apply state permissible flagging procedures.
 - (7) Advise dispatcher/carrier as appropriate.
 - (8) Record incident details (Incident Report).
 - (9) Contact property owner or appropriate public authority.
 - (10) Resolve emergency situation as quickly as possible: including, not abandoning load or leaving load unprotected, and making every effort to minimize disruption of traffic flow.
- 10. Advise team of additional procedures that may enhance public, team and load safety.

8.03.5 Assignment Conclusion

1. Turn off vehicle exterior lights.
2. Remove or cover oversize load signs and flags per state specific regulations.
3. Remove or cover amber lights per state specific regulations.
4. Remove or retract vertical clearance measuring device.

8.03.6 Quality Assurance Review/After Action Report

1. Evaluate Assignment
 - a. Were risks adequately identified?
 - b. Did the remedial measures keep the risks at an acceptable level, maximize the safe movement of the load and protect the public and the team?
 - c. Identify any unforeseen risks: including, assessing the impact of unforeseen risks, and identifying modifications to the original plan that might have remedied the shortcoming.
 - d. Review emergency incidents; including, assessing the impact on the load, team and public safety; assessing the effectiveness of emergency procedures; identifying modifications to emergency procedures; and, determining if any emergencies could have been avoided through better planning (i.e., overhead obstruction).
2. Evaluate Route Survey
 - a. Was the Route Survey accurate and beneficial to the safe movement of the load?
 - b. Did the Route Survey adequately identify potential hazards and obstructions; and if not, how could the route survey procedures be improved?
3. Evaluate the Communication Equipment and Procedures
 - a. Was the communication equipment functional?
 - b. Were the communication procedures effective and efficient, and did the procedures promote timely and accurate information distribution?
 - c. Identify modifications to the communication equipment and/or procedures to enhance effectiveness and/or efficiency.

4. Evaluate Team Dynamics
 - a. Did the team operate in an effective and efficient manner?
 - b. Did the team dynamics promote the safe movement of the load, and public and team safety?
 - c. Identify modifications to individual team member roles and responsibilities to enhance safety.
5. Communicate the results of the “Quality Assurance Review” via an “After Action Report” distributed to the motor carrier, pilot car company, and permitting official.

8.04 Pilot/Escort Vehicle Requirements:

- 8.04.1** The pilot/escort vehicle operator shall ensure that the pilot/escort vehicle is in safe operating condition at all times when the pilot/escort vehicle is escorting an extra-legal vehicle or load.
- 8.04.2** The pilot/escort vehicle shall:
1. Be either a passenger car or a two-axle truck.
 2. Not exceed a maximum gross vehicle weight rating of 14,000 pounds.
 3. Be at least 60 inches wide.
 4. Not exceed the legal limits of size and weight.
- 8.04.3** The pilot/escort operator shall not carry any item(s) or equipment or load in or on the pilot car escort vehicle which:
1. Exceeds the height, length, or width of the pilot car escort vehicle, or overhangs the pilot/escort vehicle, or otherwise impairs its immediate recognition as a safety pilot/escort vehicle by the motoring public; or
 2. Obstructs the view of the flashing yellow lights or the signs used by the pilot/escort vehicle; or
 3. Causes safety risks; or
 4. Otherwise impairs the performance by the operator or the pilot/escort vehicle of the duties required by the rules.
- 8.04.4** The pilot/escort vehicle operator shall properly load and secure any item(s) or equipment or load carried by the pilot/escort vehicle to ensure compliance with the requirements of this section.
- 8.04.5** The pilot/escort vehicle shall use its headlights at all times when escorting an extra-legal vehicle or load.

- 9.01 Scope:** The provisions of this chapter apply to longer combination vehicles (LCVs). It is intended that each state, which allows LCVs, will designate highway systems for LCV operations.

Further, the provisions of this chapter are intended as a uniformity guide for those WASHTO states that presently provide for some type of LCV operations. The inclusion of this chapter in the guide should not be interpreted as a WASHTO endorsement of LCV operations for all WASHTO states.

- 9.02 Rocky Mountain Doubles:** See figure on [page 55](#).

9.02.1 Combination Length: No maximum.

9.02.2 Combination Trailer Length: 81 feet maximum.

9.02.3 Trailer Length: 48 feet maximum.

9.02.4 Optional Dimensions: Other truck tractor double trailer combinations should be allowed which provide maximum off track and other operational characteristics similar to or better than a Rocky Mountain Doubles combination including trailers of 48 feet and 28½ feet within 81 feet combination trailer length.

- 9.03 Turnpike Doubles:** See figure on [page 55](#).

9.03.1 Combination Length: 110 feet maximum.

9.03.2 Trailer Combination Length: 95 feet maximum.

9.03.3 Trailer Length: 45 feet maximum.

9.03.4 Optional Dimensions: Other truck tractor double trailer combinations should be allowed which provide maximum off track and other operational characteristics similar to or better than a Turnpike Doubles combination with twin 45 feet trailers.

- 9.04 Triple Trailers:** See figure on [page 55](#).

9.04.1 Combination Length: No maximum.

9.04.2 Combination Trailer Length: 95 feet maximum.

9.04.3 Trailer Length: 28½ feet maximum.

9.05 Maximum Weights: Allowable axle group weights and gross vehicle weight of LCVs, should be determined in accordance with Federal Bridge Formula “B” (refer to [Chapter 2.00](#), Section 2.05.2). Maximum gross vehicle weight should be a minimum of 105,500 pounds and a maximum of 129,000 pounds.

9.06 Permits: The authority to operate LCVs should be by permit, in accordance with the LCV Model Permit Provisions contained herein.

9.07 LCV General Permit Provisions:

1. The driver and company shall be jointly responsible for meeting the permit requirements.
2. No other trailer configurations or multiple trailer combinations needing a permit may be operated under this permit unless these are specifically listed on the permit or otherwise authorized.
3. This permit requires that the permittee have an established safety program. In addition, the permittee must not have an unsafe or unsatisfactory USDOT or other safety rating from any state or province in which he operates. This permit is automatically void if the operator incurs an unsatisfactory rating. The permit will be reinstated when proof has been submitted to the permit authority that the motor carrier has been officially notified by the USDOT, the state or province giving the unsafe or unsatisfactory safety rating that a satisfactory rating has been restored.
4. The permit is valid for those routes designated on the permit. No authority is given to travel on routes other than those listed.
5. This permit may be suspended or revoked for failure to comply with any of the provisions or conditions of this permit. The motor carrier and driver, must comply with all of the motor vehicle laws and the latest motor carrier safety regulations, issued by the USDOT. Traveling faster than the posted speed limit is a permit violation.

9.08 LCV Equipment: In addition to any other requirements of this chapter, the following rules and regulations also apply:

1. All trucks and truck tractors shall be powered to provide adequate acceleration and hill-climbing ability under normal operating conditions to allow speeds compatible with other traffic. The ability to maintain a minimum speed of 20 mph while climbing grades is required unless the state or province highway engineer authorizes exemption for a specific grade.

2. Tires: Each axle must have tires of the same size and construction. Tires must be properly inflated for the load to be carried. In no event shall any tire, wheel, or rim exceed the manufacturer's maximum load-carrying limit. A vehicle designed for dual tires may not have the dual tires replaced by a single tire. No single tires on single-axle configurations, except the steering axle, are allowed.
3. All hitch connections must be of a no-slack type, which must be visible and operating. All drawbars, pickup plates, and fifth wheels must be rated to exceed the weight carried. Any kingpin must be the solid type and permanently fastened.
4. All braking systems must comply with state, province, and federal requirements. In addition, fast air transmission and release valves must be provided for all trailers, semitrailers, and converter dollies.
5. Mud flaps or splashguards must be anti-sail types. Mudguards are required behind each non-steering axle or set of axles.
6. If a sign is required, it shall read "LONG LOAD" and be at least 7 feet long and 18 inches high. The background shall be yellow with black lettering. Letters shall be at least 10 inches high with a 1.41-inch brush stroke.

Note: If Series E modified letters are used, the brush stroke shall be 2 inches.

9.09 LCV Operational Requirements:

1. Vehicles authorized under this permit must stay in the right lane when practical unless they are in the act of passing.
2. The minimum distance between the permitted vehicle and other trucks shall be at least 100 feet for each 10-mph of speed, except when passing.
3. When the speed of the vehicle drops to 20 mph less than the posted truck limit when climbing grades, the driver must use emergency flashers to notify the passing traffic that they are approaching a vehicle traveling substantially slower than the rest of the traffic.
4. Load Sequence: In no case shall any lighter trailer or semitrailer be placed ahead of a heavier trailer or semitrailer that carries more than 4,000 pounds more than a leading trailer(s). The heaviest trailer or semitrailer shall be placed in front, the lightest in the rear.
5. The operator shall turn on headlights whenever the windshield wipers are in operation or any other time when visibility is poor.

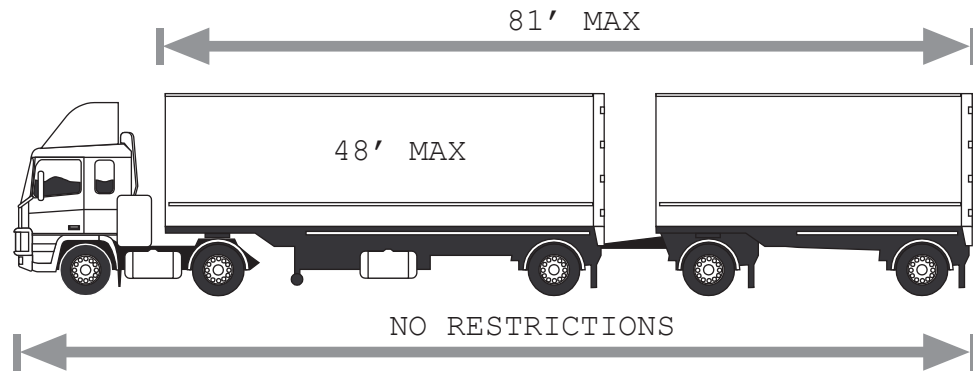
6. Extreme caution in the operation of a motor vehicle shall be exercised when hazardous conditions such as those caused by snow, ice, sleet, fog, mist rain, dust, or smoke adversely affect visibility or traction. Speed shall be reduced when such conditions exist. When conditions become sufficiently dangerous, the company or the operator shall discontinue operations, and operations shall not be resumed until the vehicle can be safely operated. The state may restrict or prohibit operations during periods when, in the state's judgment, traffic, weather, or other safety conditions make such operations unsafe or inadvisable.

9.10 LCV Driver Requirements:

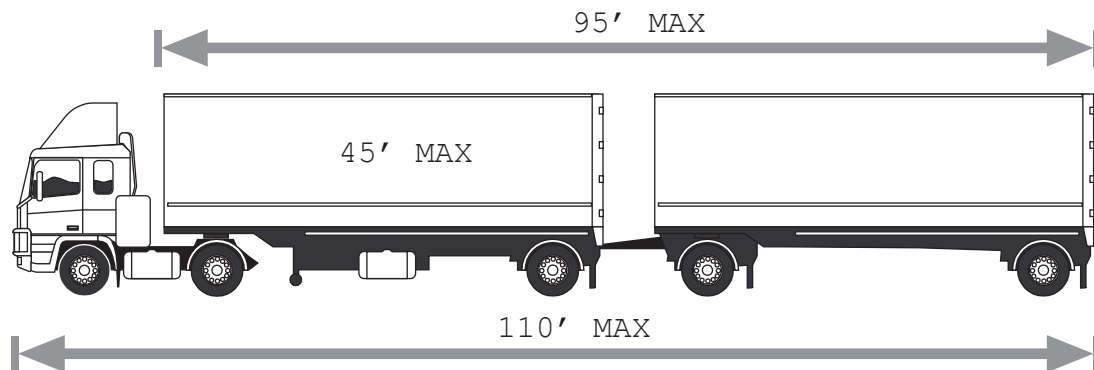
1. All LCV drivers must have a current Commercial Driver's License with the following endorsements: doubles/triples and air brake equipment.
2. LCV drivers and all other individuals or companies operating LCVs must comply with Part 391, Qualifications of Drivers of the Federal Motor Carrier Safety Regulations (FMCSR). Driver exemptions as set forth in sections 391.21 and 391.67 of the FMCSR shall not apply to LCV drivers.
3. All LCV drivers must have a minimum of two years' "linehaul" driving experience driving truck combinations. LCV triple drivers must have a minimum of two years' "line-haul" driving experience driving double-trailer combinations.
4. An LCV driver must be a regularly employed driver and supervised by the company or individual holding the LCV permit.
5. An LCV driver must have had no suspension or revocation of driving privileges in any state or province during the past 24 months where such suspension arose out of operations of a commercial motor vehicle.

Longer Combination Vehicles

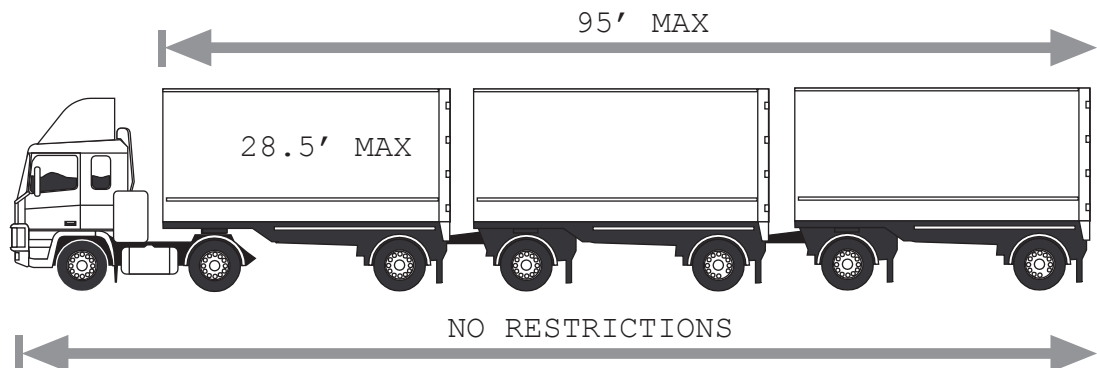
Rocky Mountain Doubles



Turnpike Doubles



Triple Trailers



10.01 Scope: It is the intent of this chapter to set forth some standards for permit programs, permit form designs, and methods of securing and paying for permits.

Adoption of these standards by the various WASHTO states should accomplish two objectives. The first, to provide greater uniformity in the permit process that would aid industry. The second, to provide a basis for eventual universal interstate permit agreements between the WASHTO states that would aid both industry and state administrations.

10.02 Single Trip Permits: All WASHTO states should provide for single trip permits.

10.03 Permit Forms: The WASHTO permit form (see figure on [pages 58 and 59](#)) should be used as a guide by individual states in developing new permit forms.

10.04 Route Identification: Routes shall be specified on each single trip permit.

10.05 Methods of Securing Permits: States should accept permit applications in person, by telephone, by wire, fax, and mail.

10.06 Methods of Payment: States should accept credit cards for payment of permit fees.

10.07 Sample Oversize/Overheight Permit:

**SPECIAL MOTOR VEHICLE
OVERSIZE/OVERWEIGHT****State Logo**

Permit Number:

Start :

Expires

Office:

Agent Name

Issued:

Report #:

Original Permit #:

Suspense #:

Company Name:

Address:

Total Fee :

Power Unit No:

License:

Power Unit Axles:

Miles

VIN

Trailer Unit Axles:

Km:

Lic State

Year:

Make:

Load/ Transportation Item:

Note:

From:

To:

Routing:

Max Weights

LB

KG

Max Dimensions

FT

Meters

Gross:

Width:

Legal:

Height:

Excess Gross:

Overall Length:

Gross Axle:

Trlr/Load Length:

Legal Axle:

Front Overhang:

Excess Axle:

Rear Overhang:

Special Conditions :

READ THIS BEFORE SIGNING

The Undersigned permittee acknowledges that the permit has been issued with the specific understanding that all applicable Washington State Laws, Administrative Codes, regulations and conditions will be complied with, and that the information contained herein is true and correct. Undersigned permittee must be contact local jurisdiction when not operating on state highways.

SIGNED X _____

PERMIT NOT VALID UNLESS SIGNED BY PERMITTEE VIOLATION OF SPECIAL PERMITS - PENALTY :

For failure to obtain, misrepresentation, or violation of special permits, State law provides for a fine, confiscation and suspension of permit without refund. No refunds after the start of the effective dates.

**SPECIAL MOTOR VEHICLE
OVERSIZE/OVERWEIGHT****SPECIAL MOTOR VEHICLE PERMIT**

Permit Number

Start Date

Expire Date

Route Nbr

MP From

MP To

ATTACHMENT

ROUTE FOR THIS PERMIT

Restriction Comments

- 11.01 Scope:** The purpose of this chapter is intended to promote safety and reduce the pavement damage resulting from operating with retractable and/or variable load suspension (VLS) axles. The following guidelines are intended to apply to divisible loads.
- 11.02 Controls:** The up-down switch may be inside the cab. The pressure regulator valve shall be positioned outside of the cab and be inaccessible from the driver's compartment.
- 11.03 Axle Rating:** Axles shall not exceed their design load capacity as indicated by an attached data plate or written certification from a vendor.
- 11.04 Tires:** Tires shall not exceed their rated load capacity (sidewall) rating as specified by the manufacturer.

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Appendix A

Vehicles in Regular Operation: National Network*

U.S. Measurement

	AK	AZ	CA	CO	HI	ID	MT	NE
Height	14'	14'	14'	13'	14'	14'	14'	14'6"
Width	8'6"	8'6"	8'6" ^H	8'6"	9'	8'6"	8'6"	8'6"
Single Unit Length	45'	40'	40' ^H	45'	45'	45'	55'	40'
Truck-Trailer								
Overall Length	75'		65' ^H	70'	65'	75'	75'	65'
Tractor-Semitrailer								
Overall Length	NS	NS	Unlimited ^H		Unlimited	NS		
Trailer Length	53'	57'6"	48'/53' ^H	57'4"		53'	53'	53'
Kingpin Distance		NS	Unlimtd/40' ^H			NS		
Tractor-Semitrailer-Trailer (Doubles)								
Overall Length	J	NS	Unlimited ^H	A	Unlimited	NS		
Combined Trailer Length	95'	NS	NS			68'	61'	65'
Individual Trailer Length	53'	28'6"	28'6" ^H	28'6"		NS	28'6"	
Weights/Inch of Tire Width (in lbs)								
Steering Axle	600#	NS	620#			600#	F	
Other Axles With Single Tires	550#		620#			600#	G	
Other Axles Without Single Tires			620#					
Axle Weights (in 1,000 lbs)								
Steering Axle	20	20	20 ^H	20		20	20	20
Single Axle	20	20	20 ^H	20	22.5	20	20	20
Tandem Axle	38	34	34 ^H	36	34	34 ^B	34	34
Gross Weight (in 1,000 lbs)								
Interstate Routes	Uncapped	80	80	80	80	80 ^C	Uncapped	80
Non-Interstate Routes			80	85		105.5		95
Federal Bridge Formula?	Yes	Yes	Yes ^H	Yes	Yes	Yes	Yes	Yes

Footnotes:

N/A..... Not Available

NS..... Not Specified™

A None unless semitrailer or trailer exceeds 28'6"

* And designated access routes

B 37,800 for special agricultural and mining products on Interstate up to GVW 79,000 lbs

C By permit on Interstate over 80,000 GVW

D Not to exceed 550 lbs per inch width of tire

E Single trailers permitted to 56', double trailers to 68'

F Manufacturers tire rating

G 500 lbs on wide base tires only

H With exceptions

I 48' semitrailer, 40' trailer

J Dimensions vary by route

NV	NM	ND	OK	OR	SD	TX	UT	WA	WY	WASHTO
14'	14'	14'	13'6"	14'	14'	14'	14'	14'	14'	14'
8'6"	8'6"	8'6"	8'6"	8'6"	8'6"	8'6"	8'6"	8'6"	8'6"	8'6"
40'	40'	50'	45'	40'	45'	45'	45'	40'	60'	45'
70'	65'	75'	70'	75'	80'	65'	65'	75'	85'	70'
	65'	Unlimited			Unlimited				NS	65'
53'	57'6"	53'	59'6"	53'	53'	59'	48'	53' ^E	60'	48'
						41'		NS		
70' ^A	65'	Unlimited			Unlimited				NS	
N/A	28'6"		110'	68'	81'6"		61'	61' ^E	81'	61'
28'6"		53'	53'	40'	45'	28'6"		53'	^I	
600#	600#	550#		600#	600#	^F	^F	600#	750#	600#
500#	600#	550#		600#	500#	^F	600#	500#	600#	500#
	550#				600#					
	20	20	20	20	20	20	20	20	20	
20	21.6	20	20	20	20	20	20	20	20	20
34	34.32	34	34	34	34	34	34	34	36	34
80	86.4	80 ^C	80 ^C	80	Uncapped ^C	80	80	105.5	117	80
		105.5	90						117	
Yes	Modified	Yes	Yes	Modified	Yes	Yes	Yes	Yes	Yes MOD	Yes

Appendix A

Vehicles in Regular Operation: National Network*

Metric Measurement

	AK	AZ	CA	CO	HI	ID	MT	NE
Height	4.26 m	4.26 m	4.26 m	3.96 m	4.26 m	4.26 m	4.26 m	4.41 m
Width	2.59 m	2.59 m	2.59 m ^H	2.59 m	2.74 m	2.59 m	2.59 m	2.59 m
Single Unit Length	13.71 m	12.19 m	12.19 m ^H	13.71 m	13.71 m	13.71 m	16.76 m	12.19 m
Truck-Trailer								
Overall Length	27.43 m		19.81 m ^H	21.33 m	19.81 m	22.86 m	22.86 m	19.81 m
Tractor-Semitrailer								
Overall Length		NS	Unlimited ^H		Unlimited	NS		
Trailer Length	16.15 m	17.52 m	14.63m/16.1m ^H	17.47 m		16.15 m	16.15 m	16.15 m
Kingpin Distance		NS	Unlimited/12.19m ^H			NS		
Tractor-Semitrailer-Trailer (Doubles)								
Overall Length	J	NS	Unlimited ^H		Unlimited	NS		
Combined Trailer Length	28.95 m	NS	NS			20.72 m	18.59 m	19.81 m
Individual Trailer Length	16.15 m	8.68 m	8.68 m ^H	8.68 m		NS	8.68 m	
Weights (kg)/Inch of Tire Width								
Steering Axle	272	NS	281			272	F	
Other Axles With Single Tires	249		281			272	G	
Other Axles Without Single Tires			281					
Axle Weights (kg)								
Steering Axle	9,071	9,071	9,071 ^H	9,071		9,071	F	
Single Axle	9,071	9,071	9,071 ^H	9,071	10,205	9,071	9,071	9,071
Tandem Axle	17,236	15,422	15,422 ^H	16,329	15,422	15,422 ^B	15,422	15,422
Gross Weight (kg)								
Interstate Routes	Uncapped	36,287	36,287	36,287	36,287	47,853 ^C	Uncapped	36,287
Non-Interstate Routes			36,287	38,556				43,091
Federal Bridge Formula?	Yes	Yes	Yes ^H	Yes	Yes	Yes	Yes	Yes

Footnotes:

N/A..... Not Available

NS..... Not Specified

m..... meters

kg..... kilograms

A None unless semitrailer or trailer exceeds 8.68 m

* And designated access routes

B 17,145 kg for special agricultural and mining products on Interstate up to GVW 35,833 kg

C By permit on Interstate over 36,287 kg GVW

D Not to exceed 249 kg per inch width of tire

E Single trailers permitted to 17.07 m, double trailers to 20.73 m

F Manufacturers tire rating

G 226 kg on wide base tires only

H With exceptions

I 14.63 m semitrailer, 12.19 m trailer

J Dimensions vary by route

NV	NM	ND	OK	OR	SD	TX	UT	WA	WY	WASHTO
4.26 m	4.26 m	4.26 m	4.11 m	4.26 m	4.26 m	4.26 m	4.26 m	4.26 m	4.26 m	4.26 m
2.59 m	2.59 m	2.59 m	2.59 m	2.59 m	2.59 m	2.59 m	2.59 m	2.59 m	2.59 m	2.59 m
12.19 m	12.19 m	15.24 m	13.71 m	12.19 m	13.71 m	13.71 m	13.71 m	12.19 m	18.28 m	13.71 m
21.33 m	19.81 m	22.86 m	21.33 m	22.86 m	24.38 m	19.81 m	19.81 m	22.86 m	25.90 m	21.33 m
	19.81 m	Unlimited							NS	19.81 m
16.15 m	17.52 m	16.15 m	18.13 m	16.15 m	16.15 m	17.98 m	14.63 m	16.15 m ^E	18.28 m	14.63 m
						12.49 m		NS		
21.33 m	19.81 m	Unlimited							NS	
N/A	8.68 m		33.52 m	20.72 m	24.84 m		18.59 m	18.59 m ^E	24.68 m	18.59 m
8.68 m		16.15 m	16.15 m	12.19 m	13.71 m	8.68 m		16.15 m	^I	
272	272	249		272	272	^F	^F	272	340	272
226	272	249		272	226	^F	272	226	272	226
	249									
	9,071	9,071	9,071	9,071	9,071	9,071	9,071	9,071	9,071	
9,071	9,797	9,071	9,071	9,071	9,071	9,071	9,071	9,071	9,071	9,071
15,422	15,567	15,422	15,422	15,422	15,422	15,422	15,422	15,422	16,329	15,422
36,287	39,190	36,287 ^C	36,287 ^C	36,287	36,287 ^C	36,287	36,287	47,853	53,070	36,287
		47,853	40,824						53,070	
Yes	Modified	Yes	Yes	Modified	Yes	Yes	Yes	Yes	Yes MOD	Yes

Appendix B

Vehicles in Regular Operation: Non-National Network

U.S. Measurement

	AK	AZ	CA	CO	HI	ID	MT	NE
Height	14'	14'	14'	13'	14'	14'	14'	14'6"
Width	8'6"	8'6"	8'6" ^R	8'6"	9'	8'6"	8'6"	8'6"
Single Unit Length	45'	40'	40' ^R	45'	45'	45'	55'	40'
Truck-Trailer								
Overall Length	75'		65' ^R	70'	65'	75'	75'	65'
Tractor-Semitrailer								
Overall Length	75'	65' ^B	65' ^R	^C	65'	65' ^O		
Trailer Length	48'	53'	Unlimited	57'4"		48'	53'	53'
Kingpin Distance	NS	NS	40'			NS		
Tractor-Semitrailer-Trailer (Doubles)								
Overall Length	120'	NS	75'	^F	65'	75' ^G		
Combined Trailer Length	95'	NS	N/A			61'	61'	65'
Individual Trailer Length	53'	28'6"	28'6" ^R	28'6"		NS	28'6"	
Weights/Inch of Tire Width								
Steering Axle	600#	NS	620#		NS	600#	^P	
Other Axles With Single Tires	550#		620#		NS	600#	^Q	
Other Axles Without Single Tires					NS			
Axle Weights (in 1,000 lbs)								
Steering Axle	20	20	20	20	20	20	^F	20
Single Axle	20	20	20	20	22.5	20	20	20
Tandem Axle	38	34	34	40 ^I	34	37.8	34	34
Gross Weight (in 1,000 lbs)								
Non-Interstate Routes	Uncapped	80	80	85	88	105.5	Uncapped	95
Federal Bridge Formula?	Yes	Yes ^R	Yes ^R	^I	^J	Yes	Yes	Yes

Footnotes:

NS..... Not Specified

B None unless the semitrailer exceeds 53'

C None unless the semitrailer exceeds 57'4"

D Single trailers permitted to 56', double trailers to 68'

E Some Non-National Network highways are 95' and 110'

F None unless semitrailer or trailer exceeds 28'6"

G None unless trailer combination length is exceeded

H 48' semitrailer, 40' trailer

I Formula: $W = 1000 \times (L+40)$

J Formula: $W = 900 \times (L+40)$ on non-Interstate

O On designated routes

P Manufacturers tire rating

Q 500 lbs wide base tires only

R With exceptions

S Based on tire size

T 80,000 lbs on posted highways

NV	NM	ND	OK	OR	SD	TX	UT	WA	WY	WASHTO
14'	14'	14'	13'6"	14'	14'	14'	14'	14'	14'	14'
8'6"	8'6"	8'6"	8'6"	8'6"	8'6"	8'6"	8'6"	8'6"	8'6"	8'6"
40'	40'	50'	45'	40'	45'	45'	45'	40'	60'	45'
70'	65'	75' ^E	70'	75'	80'	65'	65'	75'	85'	70'
70'	65' ^D	75' ^E		60'	Unlimited				NS	65'
53'	57'6"	53'	53'		53'	59'	48'	53'	60'	48'
	NS					41'		NS		
70' ^F	65' ^D	75' ^E		75'	Unlimited				NS	
	28'6"	NS	110'		81'6"		61'	61' ^D	81'	61'
28'6"		53'	53'	40'	45'	28'6"	28'6"	53'	^H	
600#	600#	550#		600#	600#	^P	^P	600#	750#	600#
500#	600#	550#		600#	500#	^P	600#	500#	600#	500#
		550#			600#					
^S	20	20	20	20	20	20	20	20	20	
20	21.6	20	20	20	20	20	20	20	20	20
34	34.32	34	34	34	34	34	34	34	36	34
80	86.4	105.5 ^T	90	80	Uncapped	80	80	105.5	117	80
Yes	Modified	Yes	Modified	Modified	Yes	Yes	Yes	Yes	Yes MOD	Yes

Appendix B

Vehicles in Regular Operation: Non-National Network

Metric Measurement

	AK	AZ	CA	CO	HI	ID	MT	NE
Height	4.26 m	4.26 m	4.26 m	3.96 m	4.26 m	4.26 m	4.26 m	4.41 m
Width	2.59 m	2.59 m	2.59 m ^R	2.59 m	2.74 m	2.59 m	2.59 m	2.59 m
Single Unit Length	13.71 m	12.19 m	12.19 m ^R	13.71 m	13.71 m	13.71 m	16.76 m	12.19 m
Truck-Trailer								
Overall Length	22.86 m		19.81 m ^R	21.33 m	19.81 m	22.86 m	22.86 m	19.81 m
Tractor-Semitrailer								
Overall Length	22.86 m	19.81 m ^B	19.81 m ^R	^C	19.81 m	19.81 m ^O		
Trailer Length	14.63 m	16.51 m	Unlimited	17.47 m		14.63 m	16.51 m	16.15 m
Kingpin Distance	NS	NS	12.19 m			NS		
Tractor-Semitrailer-Trailer (Doubles)								
Overall Length	22.86 m	NS	22.86 m	^F	19.81 m	22.86 m ^G		
Combined Trailer Length		NS	N/A			18.59 m		19.81 m
Individual Trailer Length	14.63 m	8.71 m	8.68 m ^R	8.68 m	8.68 m	NS	8.68 m	
Weights (kg)/Inch of Tire Width								
Steering Axle	272	NS	N/A		NS	272	^P	
Other Axles With or					NS			
Without Single Tires	249		N/A		NS	272	^Q	
Axle Weights (kg)								
Steering Axle	9,071	9,071	9,071	9,071	9,071	9,071	^P	9,071
Single Axle	9,071	9,071	9,071	9,071	10,205	9,071	9,071	9,071
Tandem Axle	17,236	15,422	15,422	18,143 ^I	15,422	17,145	15,422	15,422
Gross Weight (kg)	Uncapped	36,287	36,287	38,555	39,916	47,853	Uncapped	43,091
Federal Bridge Formula?	Yes	Yes ^R	Yes ^R	^I	^J	Yes	Yes	Yes

Footnotes:

m..... meters

kg..... kilograms

B None unless the semitrailer exceeds 16.15 m

C None unless the semitrailer exceeds 17.47 m

D Single trailers permitted to 17.07 m, doubles to 20.73 m

E Some Non-National Network highways are 28.95 m and 33.52

F None unless semitrailer or trailer exceeds 8.68 m

G None unless trailer combination length is exceeded

H 14.63 m semitrailer, 12.19 m trailer

I Formula: $W = 453 \text{ kg} \times (L + 12.19 \text{ m})$

J Formula: $W = 408 \text{ kg} \times (L + 12.19 \text{ m})$ on non-Interstate

O On designated routes

P Manufacturers tire rating

Q 226 kg wide base tires only

R With exceptions

S Based on tire size

T 36,287 kg GVW on posted state highways

NV	NM	ND	OK	OR	SD	TX	UT	WA	WY	WASHTO
4.26 m	4.26 m	4.26 m	4.11 m	4.26 m	4.26 m	4.26 m	4.26 m	4.26 m	4.26 m	4.26 m
2.59 m	2.59 m	2.59 m	2.59 m	2.59 m	2.59 m	2.59 m	2.59 m	2.59 m	2.59 m	2.59 m
12.19 m	12.19 m	15.24 m	13.71 m	12.19 m	13.71 m	13.71 m	13.71 m	12.19 m	18.28 m	13.71 m
21.33 m	19.81 m	22.86 m	21.33 m	22.86 m	24.38 m	19.81 m	19.81 m	22.86 m	25.90 m	21.33 m
21.33 m	19.81 m	22.86 m		18.28 m					NS	19.81 m
16.51 m	17.52 m	16.51 m	16.51 m		15.61 m	17.98 m	14.63 m	16.51 m ^D	18.28 m	14.63 m
		NS					12.49 m		NS	
21.33 m ^F	19.81 m ^D	22.86 m ^E		22.86 m					NS	
	8.68 m	NS	33.52 m		24.84 m		18.59 m	18.59 m ^D	24.68 m	18.59 m
8.68 m		15.61	16.15 m	12.19 m	13.71 m	8.68 m	8.68 m	16.51 m	^H	
272	272	249		272	272	^P	^P	272	340	272
226	272	249		272	226	^P	272	226	272	226
^S	9,071	9,071	9,071	9,071	9,071	9,071	9,071	9,071	9,071	
9,071	9,797	9,071	9,071	9,071	9,071	9,071	9,071	9,071	9,071	9,071
15,422	15,567	15,422	15,422	15,422	15,422	15,422	15,422	15,422	16,329	15,422
36,287	39,190	47,853 ^T	40,823	36,287	Uncapped	36,287	36,287	47,853	53,070	36,287
Yes	Modified	Yes	Modified	Modified	Yes	Yes	Yes	Yes	Yes MOD	Yes

Nondivisible Load Permits: National Appendix C Network and Other Designated Highways

U.S. Measurement

	AK	AZ	CA	CO	HI	ID	MT	NE
Maximum Weights								
Steering Axle:	A C							
Lb/in tire width		NS	A F	A	NS	600	A	NS
1,000s lbs			B		23			
Single Axle (1,000 lbs)	C	28	23/30 F	27	23	C	C	20
Tandem (1,000 lbs)	C	46 C	46/60 F	50	34	C	C	40
Restrictions Because of:								
Darkness	F	Yes F	Yes F	No	No	Yes	Yes	Yes
Legal Holidays	Yes	Yes F	Yes F	Yes	No	Yes	Yes	Yes F
Weekends	F	Yes F	Yes F	No	No	No F	Yes	NS
Inclement Weather	Yes F	Yes	Yes F	Yes	Yes	Yes	Yes	Yes
Escort Vehicles Required	D O	No F	Yes I	Yes F	No	Yes F	Yes	Yes F

Footnotes:

NS..... Not Specified

A Manufacturers rating

B Axle rating to 20,000 lbs; tire size over 20,000 lbs

C Depends on axle spacings and route

D Escort required if bridge restrictions of center line and 5 mph apply

E 40,495 to 46,725

F There are exceptions

G Except under special circumstances

H Only if in combination with adjacent tandem or tridem with common suspension

I If over lane width

J Unless overweight only

K 10' wide x 100' long allowed on Interstate highways at night

L 12' wide allowed on Interstate at night

M Memorial Day and Labor Day

N 14' wide x 16' high x 110' long

O Over 10'6" wide and 85' long

NV	NM	ND	OK	OR	SD	TX	UT	WA	WY	WASHTO
600	600	600		600	600	650	^A	600	750	600
			13/15 ^C						25	
24	26	24	20	21.5 ^C	31	25	29,500	22	25	24
^E	46	45	40	43 ^C	52	46	50	43	55	45
No	Yes	Yes ^J	Yes	Yes ^L	No	Yes ^{K J}	Yes	Yes ^F	No	No
No	Yes	Yes	Yes	Yes ^M	No	Yes ^N	Yes	Yes	No	No
No	Yes	Yes	Yes	Yes	No	No ^F	No	No	No	No
Yes	Yes	Yes	Yes	Yes ^M	Yes	Yes	Yes	Yes	No	No
No	Yes ^F	Yes ^F	No	No	No ^D	No ^F	Yes	Yes ^F	No ^D	No ^G

Nondivisible Load Permits: National Appendix C Network and Other Designated Highways

Metric Measurement

	AK	AZ	CA	CO	HI	ID	MT	NE
Maximum Weights (kg)								
Steering Axle:								
kg/in tire width	NS	NS	A F	A	NS	272	A	NS
Single Axle	NS	12,700	10,432 ^F 13,607 ^F	12,246	10,205	^C	^C	9,071
Tandem	NS	20,865	20,865 ^F 27,215 ^F	22,679	15,422	^C	^C	18,143
Restrictions Because of:								
Darkness	NS	No	Yes ^F	No	No	Yes	Yes	Yes
Legal Holidays	NS	No	Yes ^F	Yes	No	Yes	Yes	Yes ^F
Weekends	NS	No	Yes ^F	No	No	No ^F	Yes	NS
Inclement Weather	NS	Yes	Yes ^F	Yes	Yes	Yes	Yes	Yes
Escort Vehicles Required	NS	No ^F	Yes ^{I C}	Yes ^F	No	Yes ^F	Yes	Yes ^F

Footnotes:

- kg..... kilograms
 NS..... Not Specified
 A Manufacturers rating
 B Axle rating to 9,071 kg; tire size over 9,071 kg
 C Depends on axle spacings and route
 D Escort required if bridge restrictions of center line and 8 km per hour apply
 E 18,368 kg to 21,194 kg
 F There are exceptions
 G Except under special circumstances
 H Only if in combination with adjacent tandem or tridem with common suspension
 I If over lane width
 J Unless overweight only
 K 3.04 m wide x 30.48 m long allowed on Interstate highways at night
 L 3.65 m wide allowed on Interstate highways at night
 M Memorial Day and Labor Day
 N 4.26 m wide x 4.87 m high x 33.52 m long

NV	NM	ND	OK	OR	SD	TX	UT	WA	WY	WASHTO
272	272	272		272	272	294	^A	272		272
10,886	11,793	10,866	9,071	9,752 ^C	14,061	11,339	13,380	9,979	11,339	10,886
^E	23,586	20,411	18,143	19,504 ^C	23,586	23,586	22,679	19,504	24,947	20,411
No	Yes	Yes ^J	Yes	Yes ^L	No	Yes ^{KJ}	Yes	Yes ^F	No	No
No	Yes	Yes	Yes	Yes ^M	No	Yes ^N	Yes	Yes	No	No
No	Yes	Yes	No	Yes	No	No ^F	No	No	No	No
Yes	Yes	Yes	Yes	Yes ^M	Yes	Yes	Yes	Yes	No	No
No	Yes ^F	Yes ^F	No	No	No ^D	No ^F	Yes	Yes ^F	No ^D	No ^G

Transporting Manufactured Housing: National Appendix D Network and Other Designated Highways

U.S. Measurement

	AK	AZ	CA	CO	HI	ID	MT	NE
Dimensions								
Width	A	A	16'	A	14'	16'	18'	16' ^A
Length (inc. tongue)	A	A	80'	A		80'		
Length (inc. tow vehicle)	A	A	105'	A	70'	110'		125'
Height	A	A	A	A	A	A	A	
Tow Vehicle								
Wheelbase	100"	99"		N/A	N/A		100"	120"
Gross Vehicle Weight	10,000	B	C			D	D	12,000
Axle Requirements	J	J				J	J	J
Movement Restrictions								
Night	Yes	A	Yes ^A	Yes ^O	Yes	Yes	Yes	Yes
Weekend	Yes	A	Yes ^A	Yes ^O	Yes	No	Yes	NS
Holiday	Yes	A	Yes	Yes ^O		Yes	Yes	Yes
Inclement Weather	Yes		Yes			Yes	Yes	
Requirements								
Lights	Dot					Solid	Flash	
Lights	Beacon	Dot	Dot	Dot	Dot	Dot	Dot	
Signs	Yes	Yes	Yes	Yes	Yes	Yes	if >10'	if >10'
Pilot/Escort	if >10'6" ^X	Yes	Yes	Yes	if >12'	Yes	if >12'6"	Yes
Permits	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
Route Identification	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes

Footnotes:

- A Limited by route and/or time of day
 B 1.5 ton - 10 x 50; 2 ton - over 10 x 50
 C 6,500 lbs 8-12' wide; 9,500 lbs over 12'
 D 8,000 lbs 10-12' wide; 9,000 lbs over 12' (Idaho 12,000 lbs)
 E Plus 12" eave
 F 3/4 ton less than 18,000#; 1 1/2 ton over 18,000#
 G 3/4 ton 10' or less wide; 2 ton over 10'
 H 89" cabover 120" conventional
 I 6,000 lbs less than 10'; 8,000 lbs 10-12'; 9,000 lbs over 12'
 J 4 tires on driving axle; check with each state for required axle rating
 K Night travel on Interstate at 10' wide
 L 6,000 lbs less than 10' wide; 8,000 lbs 10-12'; 9,000 lbs over 12'
 M Yes if > 16' on Interstate or if > 20' on other highways
 N Dot and rotating if over 12' wide

NV	NM	ND	OK	OR	SD	TX	UT	WA	WY	WASHTO
14'	16'	18'	16' ^O	14'	A	A	16'	16' ^{A E W}	A	14' ^U
	85'			75'	A					80'
85'	101'	120'		80'	A	A	A	75'		90'
A	A	A	A	A	A	A	A	A	A	14' ^O
	99" >16'	100"		H			H	H		100"
F		L	G	Z	I		L	L		L
J		J		J			J	J		J
Yes	Yes	Yes	Yes	Yes ^K	Yes	Yes	Yes	Yes ^A	Daylight ^P	Daylight ^P
Yes	Yes	if >16'	No	Yes	No	No	No	No	No	
Yes	Yes	if >16'	Yes		No	Yes	Yes	Yes	Yes ^Y	Yes ^Q
	Yes	Yes	Yes		Yes	Yes	Yes	Yes	Yes	
Flash	Flash	Flash	N	N				Flash		
Dot	Warning	Dot	Dot	Flash	None	N	Flash	Flash	Flash	R
Yes	A	Yes	Yes	Yes	Yes	No	if >10'	Yes	Yes	S
Yes	if >14'	if >16'	if >12'	if >12'	Yes ^{M V}	if >16'	if >12'	if >11'	Yes ^Y	T
Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes

O..... Or limited by route

P..... 1/2 hour before sunrise to 1/2 hour after sunset

Q..... Restricted on the following holidays as well as the day before and the day after: New Year's Day, Memorial Day, Independence Day, Labor Day, Thanksgiving Day, and Christmas

R..... Flashing or rotating beacon on top of tow vehicle, plus flashing or rotating beacon on roof at rear of manufactured home if escort vehicles not required

S..... Signs meeting AASHTO guide required if manufactured home over 102" wide

T..... Escorts required when over 12' wide on 2 lane highways and over 14' wide on multi-lane highways; Escorts must be at least 60" wide and no larger than 13/4 ton pickup

U..... Plus 2' eaves

V..... Discretion of permit authority

W..... 16' base width allowed if hauled on licensed trailer

X..... Or if overall length is greater than 85 feet

Y..... 14' or > on 2 lane, 15' or > on divided

Z..... 8,000 lbs up to 11'; 15,000 lbs up to 14'; 32,000 lbs over 14'

Transporting Manufactured Housing: National Appendix D Network and Other Designated Highways

Metric Measurement

	AK	AZ	CA	CO	HI	ID	MT	NE
Dimensions								
Width	A	A	4.87 m	A	4.26 m	4.87 m	5.48 m	4.87 m ^A
Length (inc. tongue)	A	A	24.38 m	A		24.38		
Length (inc. tow vehicle)	A	A	32.00 m	A	21.33 m			38.10 m
Height	A	A	A	A	A	A	A	
Tow Vehicle								
Wheelbase	2,540 mm	2,514 mm		N/A	N/A		2.54 m	3.04 m
Gross Vehicle Weight	4,535 kg	B	C			D	L	
Axle Requirements	J	J				J	J	
Movement Restrictions								
Night	Yes	A	Yes ^A	Yes ^O	Yes	Yes	Yes	Yes
Weekend	Yes	A	Yes ^A	Yes ^O	Yes	No	Yes	NS
Holiday	Yes	A	Yes	Yes ^O		Yes	Yes	Yes
Inclement Weather	Yes		Yes			Yes	Yes	
Requirements								
Lights	Dot					Solid	Flash	
Lights	Beacon	Dot	Dot	Dot	Dot	Dot	Dot	
Signs	Yes	Yes	Yes	Yes	Yes	Yes	if>3.04 m	if>3.04 m
Pilot/Escort	if>3.04 m ^X	Yes	Yes	Yes	if>3.65 m	Yes	if>3.81 m	Yes
Permits	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
Route Identification	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes

Footnotes:

m..... meter

mm..... millimeter

kg..... kilogram

A Limited by route and/or time of day

B 1,360 kg - 3.04 m x 15.24 m; 1,814 kg - over 3.04 m x 15.24 m

C 2,948 kg 2.43-3.65 m wide; 4,309 kg over 3.65 m

D 3,628 kg 3.04 m - 3.65 m wide; 4,082 kg over 3.65 m (Idaho 5,443 kg)

E Plus .30 m eave

F 680 kg less than 8,164 kg; 1,360 kg over 8,164 kg

G 680 kg 3.04 m or less wide; 1,814 kg over 3.04 m

H 2,260 mm cabover 3,048 mm conventional

I 2,721 kg less than 3.04 m; 3,628 kg 3.04 m - 3.68 m; 4,082 kg over 3.65 m

J 4 tires on driving axle; check with each state for required axle rating

K Night travel on Interstate at 3.04 m wide

L 2,271 kg less than 3.04 m wide; 3,628 kg 3.04 m - 3.68 m; 4,082 kg over 3.65 m

M Yes if > 4.87 m on Interstate or if > 6.09 m on other highways

N Dot and rotating if over 3.65 m wide

NV	NM	ND	OK	OR	SD	TX	UT	WA	WY	WASHTO
4.26 m	4.87 m	5.48 m	4.87 m	4.26 m	A	A	4.87 m	4.87 m ^{A EW}	A	4.26 m ^U
	29.50 m			22.86 m	A					24.38 m
29.50 m	30.78 m	36.57 m		24.38 m	A	A	A	22.86 m		27.43 m
A	A	A	A	A	A	A	A	A	A	4.26 m
	8.24 m > 4.87m	8.33 m		H			H	H		8.33 m
F		L	G	Z	I		L	L		L
J		J		J			J	J		J
Yes	Yes	Yes	Yes	Yes ^K	Yes	Yes	Yes	Yes	Daylight ^P	Daylight ^P
Yes	Yes	if>4.87 m	No	Yes		No	No	No	No	
Yes	Yes	if>4.87 m	Yes	Yes		Yes	Yes	Yes	Yes ^Y	Yes ^Q
		Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	
Flash	Flash	Flash	N	N			DOT	Flash		
Dot	Warning	Dot	Dot	Flash	None	N	Flash	Flash	Flash	R
Yes	A	Yes	Yes	Yes	Yes	No	if>3.04 m	Yes	Yes	S
Yes	if>4.26 m	if>4.87 m	if>3.65 m	if>3.65 m	Yes ^{M V}	if>4.87 m	if>3.68 m	if>3.35 m	Yes ^Y	T
Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes

O..... Or limited by route

P..... 1/2 hour before sunrise to 1/2 hour after sunset

Q..... Restricted on the following holidays as well as the day before and the day after: New Year's Day, Memorial Day, Independence Day, Labor Day, Thanksgiving Day, and Christmas

R..... Flashing or rotating beacon on top of tow vehicle, plus flashing or rotating beacon on roof at rear of manufactured home if escort vehicles not required

S..... Signs meeting AASHTO guide required if manufactured home over 2,590 mm wide

T..... Escorts required when over 3.65 m wide on 2 lane highways and over 4.26 m wide on multi-lane highways; Escorts must be at least 1,524 mm wide and no larger than 1,587 kg pickup

U..... Plus .60 m eaves

V..... Discretion of permit authority

W..... 4.87 m base width allowed if hauled on licensed trailer

X..... Or if overall length is greater than 25.90 m

Y..... 4.26 m or > on 2 lane, 4.57 m or > on divided

Z..... 3,628 kg up to 3.35 m; 6,803 kg up to 4.26 m; 14,515 kg over 4.26 m

Pilot/Escort Vehicles: National Network and Other Designated Highways

Appendix E

U.S. Measurement

	AK	AZ	CA	CO	HI	ID	MT	NE
Vehicle and Load Dimensions for Which Pilot/Escort Are Required								
Two-Lane Highway								
Width	> 10'6"	12' ^I	> 10' ^A	13'	NS	12'1" ^C	12'7"	12' ^W
Length	> 85'	120'	^I	110' ^B	NS	100'	105'1"	100' ^W
Height	NS	16'		16'	NS	16'		16' ^W
Multi-Lane Highway								
Width	10'6"	14'	> 12'1" ^A	15'	NS	15'1"	16'7" ^S	12' ^W
Length	85'	120'	^I	115'	NS	120'	120'1"	100' ^W
Height	NS	16'		16'	NS	16'		16' ^W
Overhang								
Front	> 10' ^K		3' ^U		NS			
Rear	20' ^P	20'	2/3 wb	25'				
Holiday Restrictions	Yes	Yes ^J	Yes	Yes	NS	Yes ^J	Yes ^J	Yes
Manufactured Homes	Same	Same	Same	Same	NS	Same	Same	Same

Footnotes:

NS..... Not Specified

A Or when vehicle/load exceed lane width

B 85' on two-lane mountainous highways

C 10'1" on some mountainous highways

D Or second person in cab

E 12' in some situations

F Depends on load

G If over 16' on Interstate and over 20' on state system or discretion of permit authority

H 16' wide two pilot cars; 108' long two pilot cars

I Discretion of permit issuing authority

J Holiday travel allowed with reduced dimensions

K If truck crane, then overhang can be up to 18'

L 12' wide, required pilot car

M 14' one pilot car; two on all over 14'

N As required for restricted overweight move or whenever load extends to the left of centerline

O >16' one escort; > 18' two escorts

P >10'-20' may travel w/o escort if all req lighting is extended

Q New Year's Day, Memorial Day, Independence Day, Labor Day, Thanksgiving, and Christmas

R 12-14' one pilot car; 14-16' two pilot cars

S Unless highway is Interstate, the same requirements as two-lane highways

T If escorts are required, shut down 1/2 hour after sunset, day before holiday or holiday weekend
resume 1/2 hour before sunrise, day after holiday or holiday weekend

U Most vehicles limited to legal front overlong or 2/3 wheel base for cranes

V Front and rear pilot required; height pole required

W Requirement may be implemented at 8'6" wide, 14'6" high, or 90' long

NV	NM	ND	OK	OR	SD	TX	UT	WA	WY	WASHTO
12'	13'6"	if > 16'	14' ^E	N	G	> 14'	> 12'	11'	14'	R
105' +	90' +	120'	80'	95'	None	> 110'	> 105'	100'	110'	105'
15'6"	16' ^D	18'	15'9" ^V	None	None	> 17'	> 16'	14'6"	I	
14'	14' +	if > 16'	12' ^F	> 14'	16' ^G	> 14'	> 14'	14'	15'	14'
105' +	90' +	120'	80'	120'	None	> 110'	> 120'	140'	I	120'
15'6"	16'	18'	15'9"	None	None	> 17'	16'	14'6"	I	
25'				15' > (2 lane)	None	> 20'	> 20'	I		
25'				None	None	> 20'	> 20'	1/3		20'
Yes ^J	I	Yes ^J	Yes	Yes ^J	No	Yes ^{JQ}	Yes	Yes	Yes ^T	Yes ^Q
Same	Same	Same	L	M	Same	O	Same	Same	Same	Same

Pilot/Escort Vehicles: National Network and Other Designated Highways

Appendix E

Metric Measurement

	AK	AZ	CA	CO	HI	ID	MT	NE
Vehicle and Load Dimensions for Which Pilot/Escort Are Required								
Two-Lane Highway								
Width	> 3.04 m	3.65 m ^I	> 3.04 m ^A	3.96 m	NS	3.68 m ^C	3.05 m	3.65 m ^W
Length	> 25.90 m	36.57 m	^I	33.52 m ^B	NS	30.48 m	32.02 m	30.48 m ^W
Height	NS	4.87 m		4.87 m	NS	4.87 m		4.87 m ^W
Multi-Lane Highway								
Width	> 3.04 m	4.26 m	> 3.68 m ^A	4.57 m	NS	4.59 m	5.05 m ^S	3.65 m ^W
Length	> 25.80 m	36.57 m	^I	35.02 m	NS	36.57 m	36.60 m	30.48 m ^W
Height	NS	4.87 m		4.87 m	NS	4.87 m		4.87 m ^W
Overhang								
Front	> 3.04 m ^K		.91 m ^U					
Rear	6.09 m ^P	6.09 m	2/3 wb	7.62 m				
Holiday Restrictions	Yes	Yes ^J	Yes	Yes	NS	Yes ^J	Yes ^J	Yes
Manufactured Homes	Same	Same	Same	Same	NS	Same	Same	Same

Footnotes:

m..... meter

kg..... kilograms

NS..... Not Specified

A Or when vehicle/load exceed lane width

B 25.90 m on two-lane mountainous highways

C 3.07 m on some mountainous highways

D Or second person in cab

E 3.65 m in some situations

F Depends on load

G If over 4.87 m on Interstate and over 6.09 m on state system or discretion of permit authority

I Discretion of permit issuing authority

J Holiday travel allowed with reduced dimensions

K If truck crane, then overhang can be up to 5.48 m

L 3.65 m wide, required pilot car

M 4.26 m one pilot car; two on all over 4.26 m

N As required for restricted overweight move or whenever load extends to the left of centerline

O >4.87 m one escort; > 5.48 m two escorts

P >3.04 m-6.09 m may travel w/o escort if all req lighting is extended

Q New Year's Day, Memorial Day, Independence Day, Labor Day, Thanksgiving, and Christmas

R 3.65 m-4.26 m one pilot car; 4.26 m-4.87 m two pilot cars

S Unless highway is Interstate, the same requirements as two-lane highways

T If escorts are required, shut down 1/2 hour after sunset, day before holiday or holiday weekend
resume 1/2 hour before sunrise, day after holiday or holiday weekend

U Most vehicles limited to legal front overlong or 2/3 wheel base for cranes

V Front and rear pilot required; height pole required

NV	NM	ND	OK	OR	SD	TX	UT	WA	WY	WASHTO
3.65 m	4.11 m	> 4.87 m	4.26 m ^E	N	^G	> 4.26 m	> 3.65 m	3.35 m	4.26 m	^R
32.00 m	27.43 m	36.57 m	24.38 m	24.95 m		> 33.52 m	> 32.00 m	30.48 m	33.52 m	32.00 m
4.72 m	4.87 m ^D	5.48 m	5.80 m	None		> 5.18 m	4.90 m	4.41 m	^I	
4.26 m	4.26 m+	> 4.87 m	3.65 m ^F	4.26 m	4.87 m ^G	> 4.26 m	> 4.26 m	4.26 m	4.57 m	4.26 m
32.00 m +	20.43 m+	36.57 m	24.38 m	36.57 m		> 33.52 m	> 36.57 m	42.67 m	^I	36.57 m
4.72 m	4.87 m	5.48 m	5.80 m	None		> 5.18 m	4.87 m	4.41 m	^I	
7.62 m				4.57 m (2 lane)		6.09 m	> 6.09 m	^I		
7.62 m				None		6.09 m	> 6.09 m	1/3		6.09 m
Yes ^J	^I	Yes ^J	Yes	Yes ^J	No	Yes ^{J Q}	Yes	Yes	Yes ^T	Yes ^Q
Same	Same	Same	^L	m	Same	^O	^P	Same	Same	Same

Appendix F

Longer Combination Vehicles

U.S. Measurement

	AK	AZ	CO	ID	MT	NE	NV
Rocky Mountain Doubles							
Overall Length	120' ^I	105'	NS	115'	95' ^D		NS
Combined Trailer Length	95' ^I	NS	NS	A	^D	65'	NS
Semitrailer	53' ^I	48'	48'	A	NS	53'	48'
Trailer	48' ^I	NS	28'6"	A	NS	53'	42'
Turnpike Doubles							
Overall Length	120' ^I	105' ^I	NS	115'	100'	N/A	NS
Combined Trailer Length	95' ^I	NS	NS	A	NS	N/A	NS
Semitrailer	48' ^I	NS	48'	A	NS	N/A	48'
Trailer	48'	NS	48'	A	NS	N/A	42'
Triple Trailer							
Overall Length	120' ^J	105'	NS	115'	NS	N/A	NS
Combined Trailer Length	NS	NS	NS	A	95'	N/A	NS
Individual Trailers	28'6"	NS	28'6"	A	28'6"	N/A	28'6"
Maximum Gross Weight	Uncapped	^I	110	105.5	Uncapped		129
(in 1,000 lbs)	BFB	129			BFB	95	
Bridge Formula Exceptions	None	None	36	None	None	None	None
(in 1,000 lbs)			Tandem				
			Min. 6 Axles				
			Max. 9 Axles				

Footnotes:

NS..... Not Specified

N/A..... Not Allowed

IN..... Interstate

NIN Non-Interstate

m..... meter

kg..... kilogram

A Must meet off track rule max. off track for 165' radius curve 6.5' on non-Interstate routes 8.75' on Interstate routes

B 95' for gas & LP transporters

C Conv. 110'

D 81' combined trailer length; if operating under 81' combined trailer length, 95' is not applicable

E 90,000 with permit on Interstate and approved non-Interstate routes

F Uncapped Formula B for rocky mountain doubles

G Interstate and 4-lane primary highways

H Must not exceed 8.75 off track for 161' radius curve (inside front tire)

I Specified by route

J Only allowed during the summer and on one specific route

K By permit on Interstate over 80,000 lbs

L By permit on all state highways

ND	OK	OR	SD	TX	UT	WA	WY	WASHTO
110'	NS ^G		110'	N/A	NS	^L	NS	NS
NS	110'		None			68' ^L	81'	81'
53'	53'	40'	48		48'	53'	48'	48'
53'	53'		NS		48'	53'	40'	NS
110'	NS ^G	N/A	110'	N/A	NS	N/A	N/A	110'
NS	110'	N/A	None		95' IN	N/A	N/A	95'
53'	53'	N/A	48' ^H		45'	N/A	N/A	45'
53'	53'	N/A	48' ^H		45'	N/A	N/A	45'
110'	NS ^G	105'	110'	N/A	NS	N/A	N/A	NS
NS	95'	NS	None		95'	N/A	N/A	NS
53'	29'	28'6"	28'6"		28'6"	N/A	N/A	28'6"
80 ^K	80 ^E	105	129 ^F	N/A	129	105.5	117	105.5 to
105.5	90				BFB			129
9 Axles	None	None	None	N/A	None	None	36	NS
						Tandem		
Maximum								

Appendix F

Longer Combination Vehicles

Metric Measurement

	AK	AZ	CO	ID	MT	NE	NV
Rocky Mountain Doubles							
Overall Length	86.57 m	28.04 m	NS	35.00 m	28.95 m ^D		NS
Combined Trailer Length	28.95 m	NS	NS	^A	^D	19.81 m	NS
Semitrailer	16.15 m	13.71 m	14.63 m	^A	NS	16.15 m	14.63 m
Trailer	14.63 m	8.83 m	8.68 m	^A	NS	16.15 m	12.80 m
Turnpike Doubles							
Overall Length	36.57 m	32.00 m ^I	NS	35.00 m	30.48 m	N/A	NS
Combined Trailer Length	28.95 m	NS	NS	^A	NS		NS
Semitrailer	14.63 m	NS	14.63 m	^A	NS	N/A	14.63 m
Trailer	14.63 m	NS	14.63 m	^A	NS	N/A	12.80 m
Triple Trailer							
Overall Length	36.57 m ^J	32.00 m	NS	35.00 m	NS	N/A	NS
Combined Trailer Length	NS	NS	NS	^A	28.95 m	N/A	NS
Individual Trailers	8.68 m	8.68 m	8.68 m	^A	8.68 m	N/A	8.68 m
Maximum Gross Weight (kg)	Uncapped	^I	48,895	47,853	Uncapped		58,513
	BFB	56,018			BFB	43,091	
Bridge Formula Exceptions (kg)	None	None	16,329	None	None	None	None
			Tandem				
			Min. 6 Axles				
			Max. 9 Axles				

Footnotes:

NS..... Not Specified

N/A..... Not Allowed

IN..... Interstate

NIN Non-Interstate

m..... meter

kg..... kilogram

A Must meet off track rule max. off track for 50.29 m radius curve 1.98 m on non-Interstate routes 2.66 m on Interstate routes

C..... Conv. 33.52 m

D..... 24.68 m combined trailer length; if operating under 24.68 m combined trailer length, 28.95 m is not applicable

E 40,823 kg with permit on Interstate and approved non-Interstate routes

F Uncapped Formula B for rocky mountain doubles

G..... Interstate and 4-lane primary highways

H..... Must not exceed 2.66 m off track for 49.07 m radius curve (inside front tire)

I..... Specified by route

J..... Only allowed during the summer and on one specific route

K By permit on Interstate over 36,287 kg

L..... By permit on all state highways

ND	OK	OR	SD	TX	UT	WA	WY	WASHTO
33.52 m	NS ^G		NS	N/A	NS	NS	NS	NS
NS	33.52 m		24.84 m		24.68 m NIN/ 28.95 m IN	20.72 m ^L	24.68 m	24.68 m
16.15 m	16.15 m	12.19 m	13.71		14.63 m	16.15 m	14.63 m	14.63 m
16.15 m	16.15 m		NS		14.63 m	16.15 m	12.19 m	NS
33.52 m	NS ^G	N/A	33.52 m	N/A	NS	N/A	N/A	33.52 m
NS	33.52 m	N/A	NS		28.95 m	N/A	N/A	28.95 m
16.15 m	16.15 m	N/A	14.63 m		13.71 m	N/A	N/A	13.71 m
16.15 m	16.15 m	N/A	14.63 m		13.71 m	N/A	N/A	13.71 m
33.52 m	NS ^G	32 m	33.52 m	N/A	NS	N/A	N/A	NS
NS	28.95 m	NS	NS		28.95 m	N/A	N/A	NS
16.15 m	8.83 m	8.68 m	8.68 m		8.68 m	N/A	N/A	8.68 m
36,287	36,287 ^E	47,627	58,513 ^F	N/A	58,513	47,853	53,070	47,853 to
47,853	40,823				BFB			58,513
9 Axles	None	None	None	N/A	None	None	16,329	NS
Maximum							Tandem	

Appendix G Additional Longer Combination Vehicles

U.S. Measurement

	AK	AZ	CA	CO	HI	ID	MT	NE
Full Truck & Trailer								
Overall Length	P	92'	N/A		65' L	A B	95'	N/A
Truck Length	45'	NS	N/A			NS	NS	N/A
Trailer Length	53'	40'	N/A		N/A	NS	NS	N/A
Full Truck & Trailer & Trailer								
Overall Length	V	NS	N/A	N/A	N/A	B C	NS	N/A
Combined Trailer Length		NS	N/A			NS	95'	N/A
Trailer 1 Length		NS	N/A			NS	28'6"	N/A
Trailer 2 Length		NS	N/A			NS	28'6"	N/A
Dromedary Tractor & Trailer								
Overall Length	85' N	92'	N/A		N/A	B C	NS	N/A
Semitrailer Length	53'	NS	N/A	40'		NS	57'	N/A
Dromedary Tractor & Trailer & Trailer								
Overall Length	N/A	N/A	N/A	N/A	N/A	B C	O	N/A
Combined Trailer Length						NS	O	N/A
Semitrailer Length						NS	NS	N/A
Trailer Length						NS	NS	N/A
Dromedary Tractor & Trailer & Trailer & Trailer								
Overall Length	N/A	N/A	N/A	N/A	N/A	N/A	NS	N/A
Combined Trailer Length							95'	N/A
Semitrailer Length							28'6"	N/A
Trailer Length							28'6"	N/A
Truck Tractor Pole Trailer & Pole Trailer								
Overall Length	120' N	92'	N/A	N/A	N/A	A B C	95'	N/A
Combined Trailer Length	90'	NS				NS	NS	N/A
Trailer Length	48'					NS	NS	N/A
Maximum Gross Weight								
(in 1,000 lbs)	Uncapped	111,000	80,000	110,000	80,000	105,500	Uncapped	95,000
Bridge Formula Exceptions								
(in 1,000 lbs)	None	None	None	J	Q	None	None	None

Footnotes:

NS..... Not Specified

N/A..... Not Allowed

A Maximum length 85' with up to a 3' off track

B Maximum length 90' with up to a 5.5' off track

C Maximum length 115' with an off track up to 6.5' on non-Interstate routes and up to 8.75' on Interstate routes

D In a truck tractor, semitrailer, trailer configuration, combined trailer length cannot exceed 81' including connecting mechanisms; semitrailer cannot exceed 48' and the trailer cannot exceed 40'; no overall length limitations apply to this combination

E 95' on designated highways

F 110' on the National Network only

G May only operate on the Interstate and four lane divided Fed-Aid primary system; LCV permit up to 90,000 lbs on Interstate

H May operate on Interstate and four-lane highways tractor and three trailers four-lane highways are limited to a 5-mile access

I 80,000 on Interstate 90,000 off Interstate

J Formula: $L + 40 \times 800$ (L=Length) not to exceed 20,000 on single axle nor 36,000 on tandem axles

K Longer vehicle combinations may operate by permit

L No limit on allowable length on FAP highways

NV	NM	ND	OK	OR	SD	TX	UT	WA	WY
NS	N/A	75' ^{E F}	70'	75'	80' ^K	N/A	65' ^R	75'	85'
NS		50'		40'	45'				60'
NS		53'	59'6"	53'	48'		53'	53'	60'
105'	N/A	75' ^{E F}	N/A	105'	NS	N/A	95'	N/A	85'
NS		N/A	NS	NS	85'			N/A	NS
28'6"		53'	NS	U	28.5'		NS	N/A	60'
28'6"		53'	NS	U	28.5'		NS	N/A	60'
NS	N/A	75' ^{E F}	NS	75'	NS	N/A	77' ^R	NS	85'
		53'	59'6"	53'	53' ^K			53'	60'
NS	N/A	75' ^{E F}	NS	V	NS	N/A	N/A	NS	85'
NS		N/A	110' ^G	68'	81.5' ^K		N/A	68' ^S	NS
NS		53'	53'	40'	45'		53'	53'	60'
NS		53'	53'	40'	45'		53'	53'	60'
NS	N/A	75' ^{E F}	NS	N/A	N/A	N/A	N/A	N/A	N/A
NS		N/A	95' ^H	N/A	N/A		N/A	N/A	N/A
NS		53'	29'	N/A	N/A		N/A	N/A	N/A
NS		53'	29'	N/A	N/A		N/A	N/A	N/A
NS	N/A	75'	N/A	NS	NS	N/A	92'	N/A	N/A
NS		N/A	N/A	68'	81.5' ^K			68'	81' ^D
NS		53'	N/A	N/A	45'			N/A	^D
80,000		105,000	I	80,000 ^W	Uncapped		129,000	105,500	117,000
None		None	None	Modified	None		None	None	36,000 ^T

M..... Triples operation, Interstate only; combined trailer length is measured from the front of the cargo carrying unit on the truck to the rearmost portion of the second trailer, including connection tongues and load

N..... Special designated routes

O..... 95' combination length or 81' combined trailer length, provided the longer trailer does not exceed 48'

P..... Special designated routes 90' overall without permit, other designated routes 85'

Q..... 88,000 other than Interstate – Exception
W=900 (L+40)

R May go up to 85' overall with off track approval

S..... By permit on all state highways

T Per tandem

U..... Truck length limit 35'; no more than 40' trailer; no variation more than 8'

V..... Variable based on highway

W By permit to 105,500 lbs

Appendix G

Additional Longer Combination Vehicles

Metric Measurement

	AK	AZ	CA	CO	HI	ID	MT	NE
Full Truck & Trailer								
Overall Length	P	28.04 m	N/A		19.81 m ^L	A B	28.95 m	N/A
Truck Length	13.71 m	NS	N/A			NS	NS	N/A
Trailer Length	16.15 m	12.19 m	N/A		N/A	NS	NS	N/A
Full Truck & Trailer & Trailer								
Overall Length	N/A	NS	N/A	N/A	N/A	B C	NS	N/A
Combined Trailer Length		NS	N/A			NS	28.95 m	N/A
Trailer 1 Length		NS	N/A			NS	8.68 m	N/A
Trailer 2 Length		NS	N/A			NS	8.68 m	N/A
Dromedary Tractor & Trailer								
Overall Length	25.90 m ^N	28.04 m	N/A		N/A	B C	NS	N/A
Semitrailer Length	16.15 m	NS	N/A	12.19 m		NS	17.37 m	N/A
Dromedary Tractor & Trailer & Trailer								
Overall Length	N/A	N/A	N/A	N/A	N/A	B C	O	N/A
Combined Trailer Length						NS	O	N/A
Semitrailer Length						NS	NS	N/A
Trailer Length						NS	NS	N/A
Dromedary Tractor & Trailer & Trailer & Trailer								
Overall Length	N/A	N/A	N/A	N/A	N/A	N/A	NS	N/A
Combined Trailer Length							28.95 m	N/A
Semitrailer Length							8.68 m	N/A
Trailer Length							8.68 m	N/A
Truck Tractor Pole Trailer & Pole Trailer								
Overall Length	36.59 m ^N	28.04 m	N/A	N/A	N/A	A B C	28.95 m	N/A
Combined Trailer Length	27.45 m	NS				NS	NS	N/A
Trailer Length	14.63 m					NS	NS	N/A
Maximum Gross Weight								
(kg)	Uncapped	50,348	36,287	49,895	36,287	47,583	Uncapped	43,091
Bridge Formula Exceptions								
(kg)	None	None	None	J	Q	None	None	None

Footnotes:

NS..... Not Specified

N/A..... Not Allowed

kg..... kilogram

m..... meter

A Maximum length 25.90 m with up to a .91 m off track

B Maximum length 27.43 m with up to a 1.67 m off track

C Maximum length 35.00 m with an off track up to 1.98 m on non-Interstate routes and up to 2.66 m on Interstate routes

D In a truck tractor, semitrailer, trailer configuration, combined trailer length cannot exceed 24.68 m including connecting mechanisms; semitrailer cannot exceed 14.63 m and the trailer cannot exceed 12.19 m; no overall length limitations apply to this combination

E 28.95 m on designated highways

F 33.52 m on the National Network only

G May only operate on the Interstate and four lane divided Fed-Aid primary system; LCV permit up to 40,823 kg on Interstate

H May operate on Interstate and four-lane highways tractor and three trailers four-lane highways are limited to a 8.04 km access

I 36,287 kg on Interstate 40,823 kg off Interstate

J Formula: $L + 12.19 \text{ m} \times 800$ (L=Length) not to exceed 9,071 kg on single axle nor 16,329 kg on tandem axles

K Longer vehicle combinations may operate by permit

L No limit on allowable length on FAP highways

NV	NM	ND	OK	OR	SD	TX	UT	WA	WY
NS	N/A	22.86 m ^{E F}	21.33 m	31.68 m	24.38 m ^K	N/A	19.81 m ^R	22.86 m	25.90 m
NS		15.24 m		12.19 m				12.19 m	18.28 m
NS		16.15 m	18.13 m	16.15 m	NS		16.15 m	16.15 m	18.28 m
32.00 m	N/A	22.86 m ^{E F}	N/A	32.00 m	NS	N/A	28.95 m	N/A	25.90 m
NS		N/A	NS	NS	24.38 m			N/A	NS
8.68 m		16.15 m	NS	U	8.68 m		NS	N/A	18.28 m
8.68 m		16.15 m	NS	U	8.68 m		NS	N/A	18.28 m
NS	N/A	22.86 m ^{E F}	NS	31.68 m	NS	N/A	23.46 m ^R	NS	55.90 m
		16.15 m	18.16 m	16.15 m	16.15 m ^K			16.15 m	18.28 m
NS	N/A	22.86 m ^{E F}	NS	V	NS	N/A	N/A	NS	25.90 m
NS		N/A	33.52 m ^G	20.78 m	24.84 m ^K		N/A	20.72 m ^S	NS
NS		16.15 m	16.15 m	12.19 m	13.71 m		16.15 m	16.15 m	18.28 m
NS		16.15 m	16.15 m	12.19 m	13.71 m		16.15 m	18.28 m	18.28 m
NS	N/A	22.86 m ^{E F}	NS	N/A	N/A	N/A	N/A	N/A	N/A
NS		N/A	28.95 m ^H	N/A	N/A		N/A	N/A	N/A
NS		16.15 m	8.83 m	N/A	N/A		N/A	N/A	N/A
NS		16.15 m	8.83 m	N/A	N/A		N/A	N/A	N/A
NS	N/A	22.86 m	N/A	NS	NS	N/A	28.04 m	N/A	N/A
NS		N/A	N/A	20.78 m	24.84 m ^K			20.72 m	24.68 m ^D
NS		53'	N/A	N/A	13.71 m			N/A	^D
47,583		47,853	I	36,287 ^W	Uncapped		58,513	47,583	53,070
None		None	None	Modified	None		None	None	16,329 ^T

M..... Triples operation, Interstate only; combined trailer length is measured from the front of the cargo carrying unit on the truck to the rearmost portion of the second trailer, including connection tongues and load

N..... Special designated routes

O..... 28.95 m combination length or 24.68 m combined trailer length, provided the longer trailer does not exceed 14.63 m

P..... Special designated routes 27.43 m overall without permit, other designated routes 25.90 m

Q..... 39,916 kg other than Interstate – Exception
W=900 (L+12.19 m)

R May go up to 25.90 m overall with off track approval

S By permit on all state highways

T Per tandem

U..... Truck length limited to 10.66 m; no more than 12.19 m trailer; no variation greater than 2.43 m

V Variable base on highway

W By permit to 47,854 kg

Appendix H

Canadian Vehicle Weight Limits

Gross Vehicle Weights	Yukon Territories		British Columbia		Alberta	
	Kg	Lbs	Kg	Lbs	Kg	Lbs
Truck - 3 Axles	26,900	59,304	26,100	57,540	24,300	53,572
Truck - Tandem/Tandem	34,900	76,941	34,000	74,957	30,600	67,461
Truck & Pony - 6 Axles	55,500	122,356	47,100	103,837	45,300	99,869
Truck & Full - 5 Axles	43,700	96,342	44,300	97,664	42,500	93,696
Truck & Full - 7 Axles	63,300	139,552	57,100	125,883	55,300	121,660
Tractor Semitrailer - 3 Axles			24,200	53,351		
- 4 Axles			31,600	72,311		
- 5 Axles	44,000	97,003	39,500	87,081	39,500	87,081
- 6 Axles	54,000	119,049	46,500	102,513	46,500	102,513
A Train - 5 Axles			38,000	83,775	41,900	92,373
- 6 Axles			50,300	110,892	49,800	109,789
- 7 Axles	63,500	139,993	53,500	117,947	53,500	117,947
- 8 Axles	63,500	139,993	53,500	117,947	53,500	117,947
Second Trailer Weight	NR					
B Train - 5 Axles			38,000	83,775		
- 6 Axles			48,600	107,144		
- 7 Axles	63,500	139,993	56,500	124,561	56,500	124,561
- 8 Axles	63,500	139,993	63,500	139,993	63,500	139,993
C Train* - 5 Axles			38,000	83,774		
- 6 Axles			49,800	109,789		
- 7 Axles	63,500	139,993	57,700	127,206	57,700	127,206
- 8 Axles	63,500	139,993	60,500	133,378	60,500	133,379
Second Trailer Weight	NR					
Axle Loads						
Steering Axle (Truck-Tractor)	9,000	19,841	9,100	20,062	5,500	12,125
Single Axle Dual Tires	10,000	2,204	9,100	20,062	9,100	20,062
Tandem - 1.2 M Spread	16,800	37,037	17,000	37,478	17,000	37,478
Tandem - 1.8 M Spread	19,100	42,108	17,000	37,478	17,000	37,478
Tridem - 2.4 To 3.0 M Spread	22,400	46,958	24,000	52,910	21,000	46,279
Tridem - 3.0 To 3.6 M Spread	24,100	52,910	24,000	52,910	24,000	52,910
Tridem - 3.7 M Spread	24,400		24,000	52,910	24,000	52,910
Triaxle - 1.5 M + 2.5 M	27,900	61,508	NP		17,000	37,478
Triaxle - 1.8 M + 3.0 M	29,100	64,154	NP		17,000	37,478

*C Train Applicable Only to Combinations with CMVSS Approved C Dolly Converters.

Footnotes:

Blank.. Same As MOU

SP..... Special Permit Required

NR Not Regulated

NP..... Not Permitted

Lbs..... Pounds

Kg Kilograms

Saskatchewan		Manitoba		NW Territories		MOU	
Kg	Lbs	Kg	Lbs	Kg	Lbs	Kg	Lbs
		24,300	53,572			22,500	49,604
28,000	61,729	SP		39,500	87,081	NP	
		45,300	99,869	46,500	102,513	43,500	95,901
40,700	89,728	41,300	91,050			39,500	87,082
		55,300	121,915			53,500	117,947
					23,700	52,249	
						31,600	69,666
						39,500	87,082
						46,500	102,514
41,900	92,373	41,900	92,373			39,700	87,523
48,800	109,789	48,800	107,584			47,600	104,940
		55,500	122,356			53,500	117,947
		55,500	122,356			53,500	117,947
NR						16,000	35,273
						40,700	89,728
						48,600	107,144
						56,500	124,561
						62,500	137,788
					41,900	92,373	
						49,800	109,790
53,500	117,947	60,500	133,379	57,700	127,205	54,600	120,372
		60,500	133,379			58,500	128,970
NR						21,000	46,297
						5,500	12,125
						9,100	20,062
						17,000	37,478
						17,000	37,478
						21,000	46,297
						23,000	50,706
						24,000	52,910
NP		NP		NR		NP	
NP		NP		NR		NP	

Appendix I

Canadian Dimension Limits

	Yukon Territories		British Columbia		Alberta	
	M	FT	M	FT	M	FT
Overall Height (Max)	4.20	13.77	4.15	13.61	4.15	13.61
Overall Width (Max)	2.60	8.50	2.60	8.50	2.60	8.50
Overall Length (Max)			25.00	82.02		
Straight Truck						
Truck & Full Trailer	22.00	72.17	23.00	75.45	23.00	75.45
Truck & Pony Trailer			23.00	75.45	23.00	75.45
Tractor Semitrailer	23.00	72.17	23.00	75.45	23.00	75.45
A Train	25.00	82.02	25.00	82.02	25.00	82.02
B Train	25.00	82.02	25.00	82.02	25.00	82.02
C Train	25.00	82.02	25.00	82.02	25.00	82.02
Trailer Length (Max)						
Full Trailer	16.20	44.29	12.50	41.01	12.50	41.01
Semitrailer	16.20	44.29	16.20	53.14	16.20	53.14
Box Length (Max)						
Truck & Full Or Pony Trailer	NR		20.00	65.61	20.00	65.61
A Train	NR		20.00	65.61	20.00	65.61
B Train	NR		20.00	65.61	20.00	65.61
C Train	NR		20.00	65.61	20.00	65.61
Effective Rear Overhang						
Straight Truck (Max)			4.00	13.12	4.00	13.12
Semitrailer (Max)			35% of WB		35% of WB	
Full Trailer (Max)			35% of WB			
Pony Trailer (Max)			4.00	13.12	4.00	13.12
Wheelbase						
Tractor (Max)	NR		6.20	20.34	6.20	20.34
Full Trailer (Min)	NR		6.25	20.50	6.25	20.50
Semitrailer (Max)	NR		12.50	41.01	12.50	41.01
Semitrailer (Min)	NR		6.25	20.50	6.25	20.50
Semitrailer With Tridem (Min)	NR		6.25	20.50	6.25	20.50

Footnotes:

N/A..... Not Available

Blank.. Same As MOU (what is MOU?)

MOU .. Memorandum of Understanding between Provinces

SP..... Special Permit Required

NR Not Regulated

NP..... Not Permitted

M..... Meters

FT Feet

WB..... Wheelbase

Saskatchewan		Manitoba		NW Territories		MOU	
M	FT	M	FT	M	FT	M	FT
	4.15		4.20		4.15	13.61	
		2.60	8.50	2.60	8.50	2.60	8.50
		25.00	82.02	26.00	85.30		
						12.50	41.01
				21.00		23.00	75.45
				21.00	75.45	23.00	75.45
				25.00		23.00	75.45
25.00	82.02	25.00	82.02	25.00		23.00	75.45
25.00	82.02	25.00	82.02	25.00		23.00	75.45
25.00	82.02	25.00	82.02	25.00		23.00	75.45
16.20	53.14			NR		12.50	41.01
16.20	53.14	16.20	53.14	NR		16.20	53.14
				NR		20.00	65.62
		20.00	65.62	NR		18.50	60.69
		20.00	65.62	NR		20.00	65.62
		20.00	65.62	NR		20.00	65.62
NR		4.00		NR		4.00	13.12
						35% of WB	
4.10		NR		35% of WB	35% of WB	NR	
4.10				35% of WB	35% of WB	4.00	13.12
						6.20	20.34
		6.25	20.50	5.00	16.40	6.50	21.32
				NR		12.50	41.01
		6.25	20.50	5.00	16.40	6.50	21.32
6.50	21.32	6.25	20.50	5.50	18.04	9.50	31.16

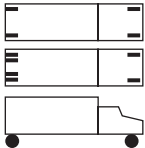
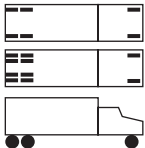
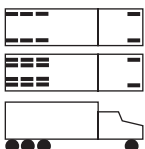
Appendix J

Mexican Vehicle Weight Limits

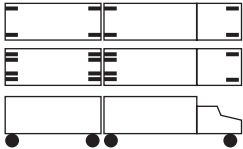
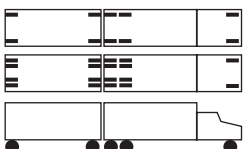
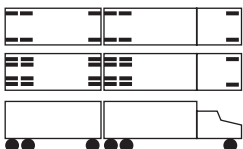
			Tipo De Camino / Road Type	
Configuration De Ejes Vehicle Configurations			A4 Y A2 Metric Tons	Pounds
I—I 0	Sencillo Dos Llantas	Single Axle Single Tires	6.50	14,330
I—I 0	Motriz Sencillo Dos Llantas	Drive Axle Single Tires	7.00	15,432
II—II 0	Sencillo Cuatro Llantas	Single Axle Dual Tires	10.00	22,046
II—II 0	Motriz Sencillo Cuatro Llantas	Drive Axle Dual Tires	11.00	24,250
I—I 00 I—I	Doble O Tandem Cuatro Llantas	Dual Axle Single Tires	11.00	24,250
I—I 00 I—I	Motriz Doble O Tandem Cuatro Llantas	Dual Drive Axle Single Tires	12.50	27,557
II—II 00 I—I	Doble O Tandem Seis Llantas	Dual Axle 6 Tires	14.50	31,967
II—II 00 I—I	Motriz Doble O Tandem Seis Llantas	Dual Drive 6 Tires	15.50	34,171
II—II 00 II—II	Doble O Tandem Ocho Llantas	Dual Axle Dual Tires	18.00	39,683
II—II 00 II—II	Motriz Doble O Tandem Ocho Llantas	Dual Drive Axle Dual Tires	19.50	42,990
I—I 000 I—I I—I	Triple O Tridem Seis Llantas	Triple Axle Single Tires	14.00	30,864
I—I 000 I—I I—I	Motriz Triple O Tridem Seis Llantas	Triple Drive Axle Single Tires	15.50	34,171
II—II 000 II—II II—II	Triple O Tridem Doce Llantas	Triple Axle Dual Tires	22.50	49,604
II—II 000 II—II II—II	Motriz Triple O Tridem Doce Llantas	Triple Drive Axle Dual Tires	24.50	54,013

Tipo De Camino / Road Type					
B4 Y B2 Metric Tons	Pounds	C Metric Tons	Pounds	D Metric Tons	Pounds
6.50	14,330	5.50	12,125	5.00	11,023
7.00	15,432	6.00	13,227	5.50	12,125
10.00	22,046	9.00	19,841	8.00	17,636
11.00	24,250	10.00	22,046	9.00	19,841
11.00	24,250	10.00	22,046	9.00	19,841
12.50	27,557	11.00	24,250	10.00	22,046
14.50	31,967	13.00	28,660	11.50	25,353
15.50	34,171	14.00	30,864	12.50	27,557
18.00	39,683	16.00	35,273	14.00	30,864
19.50	42,990	17.50	38,580	15.50	34,171
14.00	30,864	12.50	41,017	11.50	37,723
15.50	34,171	14.00	30,864	12.50	27,557
22.50	49,604	20.00	44,092	18.00	39,683
24.50	54,010	22.00	48,501	19.50	42,990

Camion / Full Truck

Configuracion Del Vehiculo Vehicle Configurations	Numero De Llantas	Number of Tires	Tipo De Camino / Road Type	
			A4 Y A2 Metric Tons	Pounds
C2 (4/6) 	Cuatro Llantas	4	13.50	29,762
	Seis Llantas	6	17.50	38,580
C3 (6/10) 	Seis Llantas	6	19.00	41,887
	Diez Llantas	10	26.00	57,320
C4 (8/14) 	Ocho Llantas	8	22.00	48,501
	Catorce Llantas	14	31.00	68,343

Camion Remolque / Full Truck and Trailer

Configuracion Del Vehiculo Vehicle Configurations	Numero De Llantas	Number of Tires	Tipo De Camino / Road Type	
			A4 Y A2 Metric Tons	Pounds
C2-R2 (8/14) 	Ocho Llantas	8	26.50	58,422
	Catorce Llantas	14	37.50	82,673
C3-R2 (10/18) 	Diez Llantas	10	32.00	70,547
	Dieciocho Llantas	18	46.00	101,412
C3-R3 (12/22) 	Doce Llantas	12	36.50	80,468
	Vientidos Llantas	22	54.00	119,049

Footnotes:

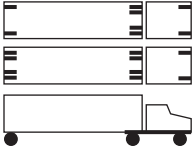
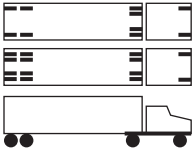
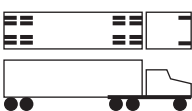
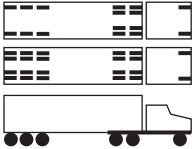
El Peso Bruto Vehicular (Pbv) Maximo Autorizado Podra Incrementarse Hasta Un Cinco Por ciento (5%), Si El Vehiculo Esta Equipado Con Suspension Neumatic O Equivalente En Todos Sus Ejes, Excepto En El Eje Direccional.

Tipo De Camino / Road Type					
B4 Y B2 Metric Tons	Pounds	C Metric Tons	Pounds	D Metric Tons	Pounds
13.50	29,762	11.50	25,353	11.00	24,250
17.50	38,580	15.50	34,171	14.00	30,864
19.00	41,887	16.50	36,376	15.00	33,069
26.00	57,320	23.00	50,706	20.50	45,194
22.00	48,501	19.50	42,990	17.50	38,580
31.00	68,343	27.50	60,627	24.50	54,013

Tipo De Camino / Road Type					
B4 Y B2 Metric Tons	Pounds	C Metric Tons	Pounds	D Metric Tons	Pounds
26.50	58,422	22.50	49,604	N/A	N/A
37.50	82,673	33.50	73,854	N/A	N/A
32.00	70,547	27.50	60,627	N/A	N/A
46.00	101,412	41.00	90,389	N/A	N/A
36.50	80,468	32.00	70,547	N/A	N/A
54.00	119,049	48.00	105,821	N/A	N/A

The gross vehicle weight authorized can be increased 5% if the vehicle is equipped with pneumatic suspension or equivalent on all its axles except the steer axle.

Tractocamion Articulado / Truck Tractor and Trailer

Configuracion Del Vehiculo Vehicle Configurations	Numero De Llantas	Number of Tires	Tipo De Camino / Road Type	
			A4 Y A2 Metric Tons	Pounds
T2-S1 (8/10) 	Ocho Llantas	8	24.00	52,910
	Diez Llantas	10	27.50	60,627
T2-S2 (10/14) 	Diez Llantas	10	28.50	62,831
	Catorce Llantas	14	35.50	78,264
T3-S2 (18) 	Dieciocho Llantas	18	44.00	97,003
T3-S3 (16/22) 	Dieciseis Llantas	16	40.00	88,184
	Veintidos Llantas	22	48.50	88,184

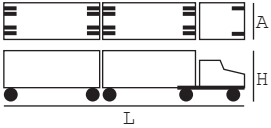
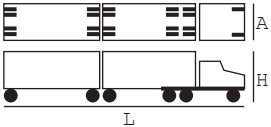
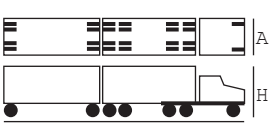
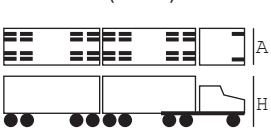
Footnotes:

El peso bruto vehicular (pbv) maximo autorizado podra incrementarse hasta un cinco porciento (5%), si el vehiculo esta equipado con suspension neumatic o equivalente en todos sus ejes, excepto en el eje direccional.

The gross vehicle weight authorized can be increased 5% if the vehicle is equipped with pneumatic suspension or equivalent on all its axles except the steer axle.

Tipo De Camino / Road Type					
B4 Y B2 Metric Tons	Pounds	C Metric Tons	Pounds	D Metric Tons	Pounds
24.00	52,910	21.00	46,297	N/A	N/A
27.50	60,627	24.50	54,013	N/A	N/A
28.50	62,831	25.50	56,217	N/A	N/A
35.50	78,264	31.50	69,445	N/A	N/A
44.00	97,003	39.00	85,980	N/A	N/A
40.00	88,184	35.50	78,264	N/A	N/A
48.50	106,924	43.00	94,798	N/A	N/A

Tractocamion Doblemente Articulado / Truck Tractor and Trailer and Trailer

			Tipo De Camino / Road Type	
Configuracion Del Vehiculo Vehicle Configurations	Numero De Llantas	Number of Tires	A4 Y A2 Metric Tons	Pounds
T2-S1-R2 (12/18) 	Doce Llantas	12	37.00	81,571
	Dieciocho Llantas	18	47.50	104,719
T3-S1-R2 (16/22) 	Dieciseis Llantas	16	45.50	100,310
	Veintidos Llantas	22	56.00	123,458
T3-S2-R2 (18/26) 	Diciocho Llantas	18	50.00	110,231
	Veintiseis Llantas	26	60.50	133,379
T3-S2-R4 (22/34) 	Veintidos Llantas	22	59.00	130,072
	Treinta Y Cuatro Llantas	34	66.5*	146,607

Footnotes:

El peso bruto vehicular (pbv) maximo autorizado podra incrementarse hasta un cinco porciento (5%), si el vehiculo esta equipado con suspension neumatic o equivalente en todos sus ejes, excepto en el eje direccional.

The gross vehicle weight authorized can be increased 5% if the vehicle is equipped with pneumatic suspension or equivalent on all its axles except the steer axle.

* el peso bruto vehicular para este tipo de unidades que trasladan gases o quimicos por caminos tipo "a" sera de 72.5 ton.

Por un periodo do 5 anos a partir de la expedicion del presente reglamento, posteriormente a este periodo deberan ajustarse al valor endicado.

The unit's gross vehicle weight, that transports gases and chemicals, over road type a would be 72.5 ton.

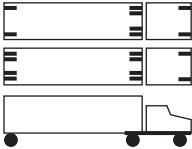
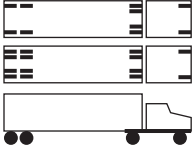
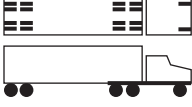
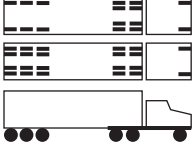
For a period of 5 years as of this rule, before this rule you should adjust the weights indicated.

Tipo De Camino / Road Type					
B4 Y B2 Metric Tons	Pounds	C Metric Tons	Pounds	D Metric Tons	Pounds
37.00	81,571	32.00	70,547	N/A	N/A
47.50	104,719	42.50	93,696	N/A	N/A
45.50	100,310	39.50	87,082	N/A	N/A
55.00	121,254	50.00	110,231	N/A	N/A
50.00	110,231	44.00	97,003	N/A	N/A
58.00	127,868	52.50	115,742	N/A	N/A
59.00	130,072	53.00	116,845	N/A	N/A
64.00	141,095	58.00	127,868	N/A	N/A

Appendix K

Mexican Vehicle Dimension Limits

Tractocamion Articulado / Truck Tractor and Trailer

			Tipo De Camino / Road Type	
Configuracion Del Vehiculo Vehicle Configurations	Tipo De Vehiculo	Type of Vehicle	A4 Y A2 Metric	Feet
T2-S1 (8/10) 	Tracto Camion Semir Remolque	Truck Tractor & Trailer	A = 2.50 L = 19.50	W = 8.20 L = 63.97
	Tres Ejes	3 Axles	A = 2.60 L = 19.00	W = 8.53 L = 62.33
T2-S2 (10/14) 	Tracto Camion Semir Remolque	Truck Tractor & Trailer	A = 2.50 L = 19.50	W = 8.20 L = 63.97
	Cuatro Ejes	4 Axles	A = 2.60 L = 19.00	W = 8.53 L = 62.33
T3-S2 (18) 	Tracto Camion Semir Remolque	Truck Tractor & Trailer	A = 2.50 L = 19.50	W = 8.20 L = 63.97
	Cinco Ejes	5 Axles	A = 2.60 L = 19.00	W = 8.53 L = 62.33
T3-S3 (16-22) 	Tracto Camion Semir Remolque	Truck Tractor & Trailer	A = 2.50 L = 19.50	W = 8.20 L = 63.97
	Cinco Ejes	6 Axles	A = 2.60 L = 19.00	W = 8.53 L = 62.33

Footnotes:

A Ancho Del Vehiculo

L Longitud Del Vehiculo

H Altura Del Vehiculo 4.15 M


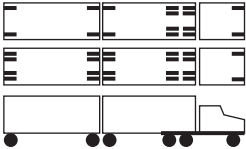
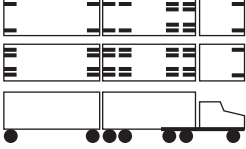
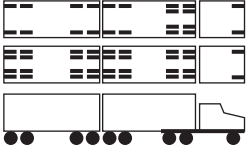
W Width of Vehicle

L Length of Vehicle

H Height 13.61 Ft

Tipo De Camino / Road Type					
B4 Y B2 Metric	Feet	C Metric	Feet	D Metric	Feet
A = 2.50 L = 18.00	W = 8.20 L = 59.05	A = 2.50 L = 16.50	W = 8.20 L = 54.13	N/A	N/A
A = 2.60 L = 17.00	W = 8.53 L = 55.77	A = 2.60 L = 16.00	W = 8.53 L = 52.49	N/A	N/A
A = 2.50 L = 18.00	W = 8.20 L = 59.05	A = 2.50 L = 16.50	W = 8.20 L = 54.13	N/A	N/A
A = 2.60 L = 17.00	W = 8.53 L = 55.77	A = 2.60 L = 16.00	W = 8.53 L = 52.49	N/A	N/A
A = 2.50 L = 18.00	W = 8.20 L = 59.05	A = 2.50 L = 16.50	W = 8.20 L = 54.13	N/A	N/A
A = 2.60 L = 17.00	W = 8.53 L = 55.77	A = 2.60 L = 16.00	W = 8.53 L = 52.49	N/A	N/A
A = 2.50 L = 18.00	W = 8.20 L = 59.05	A = 2.50 L = 16.50	W = 8.20 L = 54.13	N/A	N/A
A = 2.60 L = 17.00	W = 8.53 L = 55.77	A = 2.60 L = 16.00	W = 8.53 L = 52.49	N/A	N/A

Tractocamion Doblemente Articulado / Truck Tractor and Trailer and Trailer

			Tipo De Camino / Road Type	
Configuracion Del Vehiculo Vehicle Configurations	Tipo De Vehiculo	Type of Vehicle	A4 Y A2 Metric	Feet
T2-S1-R2 (12/18) 	Tracto Camion Articulado Multiple	Truck Tractor Trailer & Trailer	A = 2.50 L = 28.50	W = 8.20 L = 93.50
	Cinco Ejes	5 Axles	A = 2.60 L = 27.50	W = 8.53 L = 90.22
T3-S1-R2 (16/22) 	Tracto Camion Articulado Multiple	Truck Tractor Trailer & Trailer	A = 2.50 L = 28.50	W = 8.20 L = 93.50
	Seis Ejes	6 Axles	A = 2.60 L = 27.50	W = 8.53 L = 90.22
T3-S2-R2 (18/26) 	Tracto Camion Articulado Multiple	Truck Tractor Trailer & Trailer	A = 2.50 L = 28.50	W = 8.20 L = 93.50
	Seite Ejes	7 Axles	A = 2.60 L = 27.50	W = 8.53 L = 90.22
T3-S2-R4 (22/34) 	Tracto Camion Articulado Multiple	Truck Tractor Trailer & Trailer	A = 2.50 L = 28.50	W = 8.20 L = 93.50
	Nueve Ejes	9 Axles	A = 2.60 L = 27.50	W = 8.53 L = 90.22

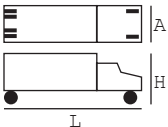
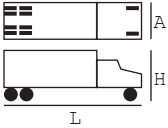
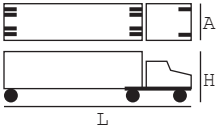
Footnotes:

A Ancho Del Vehiculo
 L Longitud Del Vehiculo
 H Altura Del Vehiculo 4.15 M

W Width of Vehicle
 L Length of Vehicle
 H Height 13.61 Ft

Tipo De Camino / Road Type					
B4 Y B2 Metric	Feet	C Metric	Feet	D Metric	Feet
A = 2.50 L = 24.00	W = 8.20 L = 78.74	A = 2.50 L = 23.50	W = 8.20 L = 77.09	N/A	N/A
A = 2.60 L = 23.00	W = 8.53 L = 75.45	A = 2.60 L = 22.50	W = 8.53 L = 73.81	N/A	N/A
A = 2.50 L = 24.00	W = 8.20 L = 78.74	A = 2.50 L = 23.50	W = 8.20 L = 77.09	N/A	N/A
A = 2.60 L = 23.00	W = 8.53 L = 75.45	A = 2.60 L = 22.50	W = 8.53 L = 73.81	N/A	N/A
A = 2.50 L = 24.00	W = 8.20 L = 78.74	A = 2.50 L = 23.50	W = 8.20 L = 77.09	N/A	N/A
A = 2.60 L = 23.00	W = 8.53 L = 75.45	A = 2.60 L = 22.50	W = 8.53 L = 73.81	N/A	N/A
A = 2.50 L = 24.00	W = 8.20 L = 78.74	A = 2.50 L = 23.50	W = 8.20 L = 77.09	N/A	N/A
A = 2.60 L = 23.00	W = 8.53 L = 75.45	A = 2.60 L = 22.50	W = 8.53 L = 73.81	N/A	N/A

Camion / Full Truck

			Tipo De Camino / Road Type	
Configuracion Del Vehiculo Vehicle Configurations	Tipo De Vehiculo Vehicle	Type of Vehicle	A4 Y A2 Metric	Feet
C2 (4/6) 	Camion	Full Truck	A = 2.50 L = 14.00	W = 8.20 L = 45.93
	Dos Ejes	2 Axles	A = 2.60 L = 13.71	W = 8.53 L = 44.98
C3 (6/10) 	Camion	Full Truck	A = 2.50 L = 14.00	W = 8.20 L = 45.93
	Tres Ejes	3 Axles	A = 2.60 L = 13.71	W = 8.53 L = 44.98
C4 (8/14) 	Camion	Full Truck	A = 2.50 L = 14.00	W = 8.20 L = 45.93
	Cuatro Ejes	4 Axles	A = 2.60 L = 13.71	W = 8.53 L = 44.98

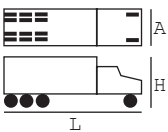
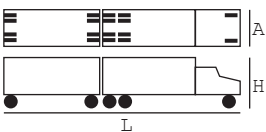
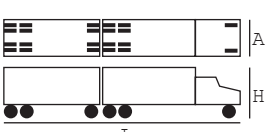
Footnotes:

A Ancho Del Vehiculo
 L..... Longitud Del Vehiculo
 H..... Altura Del Vehiculo 4.15 M

W Width of Vehicle
 L Length of Vehicle
 H..... Height 13.61 Ft

Tipo De Camino / Road Type					
B4 Y B2 Metric	Feet	C Metric	Feet	D Metric	Feet
A = 2.50 L = 14.00	W = 8.20 L = 45.93	A = 2.50 L = 14.00	W = 8.20 L = 45.93	A = 2.50 L = 12.50	W = 8.20 L = 41.01
A = 2.60 L = 17.00	W = 8.53 L = 55.77	A = 2.60 L = 16.00	W = 8.53 L = 52.49	A = 2.60 L = 12.25	W = 8.53 L = 40.19
A = 2.50 L = 18.00	W = 8.20 L = 59.05	A = 2.50 L = 16.50	W = 8.20 L = 54.13	A = 2.50 L = 12.50	W = 8.20 L = 41.01
A = 2.60 L = 17.00	W = 8.53 L = 55.77	A = 2.60 L = 16.00	W = 8.53 L = 52.49	A = 2.60 L = 12.25	W = 8.53 L = 40.19
A = 2.50 L = 18.00	W = 8.20 L = 59.05	A = 2.50 L = 16.50	W = 8.20 L = 54.13	A = 2.50 L = 12.50	W = 8.20 L = 41.01
A = 2.60 L = 17.00	W = 8.53 L = 55.77	A = 2.60 L = 16.00	W = 8.53 L = 52.49	A = 2.60 L = 12.25	W = 8.53 L = 40.19

Camion Remolque / Full Truck and Trailer

			Tipo De Camino / Road Type	
Configuracion Del Vehiculo Vehicle Configurations	Tipo De Vehiculo Vehicle	Type of Vehicle	A4 Y A2 Metric	Feet
C2-R2 (8/14) 	Camion Remolque	Full Truck	A = 2.50 L = 28.50	W = 8.20 L = 93.50
	Cuatro Ejes	4 Axles	A = 2.60 L = 27.00	W = 8.53 L = 88.58
C3-R2 (10/18) 	Camion Remolque	Full Truck	A = 2.50 L = 28.50	W = 8.20 L = 93.50
	Cinco Ejes	5 Axles	A = 2.60 L = 27.00	W = 8.53 L = 88.58
C4-R3 (12/22) 	Camion Remolque	Full Truck	A = 2.50 L = 28.50	W = 8.20 L = 93.50
	Seis Ejes	6 Axles	A = 2.60 L = 27.00	W = 8.53 L = 88.58

Footnotes:

A Ancho Del Vehiculo
 L Longitud Del Vehiculo
 H Altura Del Vehiculo 4.15 M

W Width of Vehicle
 L Length of Vehicle
 H Height 13.61 Ft

Tipo De Camino / Road Type					
B4 Y B2 Metric	Feet	C Metric	Feet	D Metric	Feet
A = 2.50 L = 23.50	W = 8.20 L = 77.09	A = 2.50 L = 22.50	W = 8.20 L = 73.81	N/A	N/A
A = 2.60 L = 27.00	W = 8.53 L = 73.81	A = 2.60 L = 21.50	W = 8.53 L = 70.53	N/A	N/A
A = 2.50 L = 23.50	W = 8.20 L = 77.09	A = 2.50 L = 22.50	W = 8.20 L = 73.81	N/A	N/A
A = 2.60 L = 27.00	W = 8.53 L = 73.81	A = 2.60 L = 21.50	W = 8.53 L = 70.53	N/A	N/A
A = 2.50 L = 23.50	W = 8.20 L = 77.09	A = 2.50 L = 22.50	W = 8.20 L = 73.81	N/A	N/A
A = 2.60 L = 27.00	W = 8.53 L = 73.81	A = 2.60 L = 21.50	W = 8.53 L = 70.53	N/A	N/A

RESOLUTION OF THE WESTERN ASSOCIATION OF STATE HIGHWAY
AND TRANSPORTATION OFFICIALS

REGARDING

ADOPTING THE SIXTH EDITION OF “A GUIDE FOR UNIFORM
LAWS AND REGULATIONS GOVERNING TRUCK SIZE AND WEIGHT
AMONG THE WASHTO STATES”

WHEREAS, Resolution 1-90, approved by the WASHTO Policy Committee,
April 6, 1990, adopted the first edition of the “GUIDE”; and

WHEREAS, Resolution 1-90 recognized that the “GUIDE” should be a
continuing and dynamic document; and

WHEREAS, Resolution 7-93, 7-95, 3-97, and 4-99 approved by the WASHTO
Policy Committee, adopted the second, third, fourth, and fifth editions to the
“GUIDE” and, accordingly, is proposing a sixth edition;

NOW, THEREFORE, BE IT RESOLVED that the WASHTO Policy
Committee hereby adopts this sixth edition of “A GUIDE FOR UNIFORM
LAWS AND REGULATIONS GOVERNING TRUCK SIZE AND WEIGHT
AMONG THE WASHTO STATES” (Changes summarized in Attachment A);
and

BE IT FURTHER RESOLVED that the WASHTO Policy Committee
continues to support the work of the Committee on Highway Transport to
resolve highway transportation issues in the WASHTO member states and
to propose further updates and amendments to the “GUIDE” as deemed
necessary; and

BE IT FURTHER RESOLVED that the WASHTO Policy Committee
continues to encourage the individual states to incorporate, to the extent
possible, the recommendations of the “GUIDE” into laws,
regulations and policies of all the WASHTO states in order to accomplish
increased efficiency of interstate truck transportation in the WASHTO region.

Adopted by the WASHTO Policy Committee in June 2004.